

Memo

To: Ed Sconfienza, The Site Group
From: Brian Chambers, Senior Planner; 910.342.2782
CC: File;
Date: 8/10/2020
Re: The Healing Place TRC Rev. 1

The following is a list of comments for review from planning regarding the project. Please provide your corrections as listed below. A staff summary of comments:

Staff	Department	Notes
Brian Chambers	Planning, Plan Review	Comments below
Trent Butler	Engineering	Comments attached
Chris Walker	Fire	Comments attached
Mitesh Baxi	Traffic Engineering	Comments attached
Bill McDow	Transportation	Comments attached
Anna Reh-gingerich	Stormwater	Comments attached

Planning Review

Brian Chambers, brian.chambers@wilmingtonnc.gov, 910.342.2782

Comments:

- Provide copy of wetland determination.
- Provide copy of wetland impact permit.
- Show required conservation resource setback for wetlands not impacted by fill permit.
- Provide soil types on existing conditions sheet.
- Provide dumpster screening detail
- Provide streetyard calculations in a table on the landscape plan, including required and proposed number of plantings.
- Provide foundation planting areas and calculations for all building facades facing internal parking areas and drive isles.

Engineering has reviewed the TRC submittal for The Healing Place and offers the following comments:

1. Please submit a copy of the revised State stormwater permit to Engineering once received.
2. A few proposed trees near the Dining/Shelter/Storage and Men's Residential buildings conflict with proposed stormwater pipe locations. Please adjust the locations of these trees.
3. The impervious area totals listed in the Site Data Summary on sheet SITE 001 do not match what is shown on the stormwater permit application. Please revise.

Project Name: **The Healing Place**

Formal TRC Date: August 13, 2020

Reviewer Name: Chris Walker

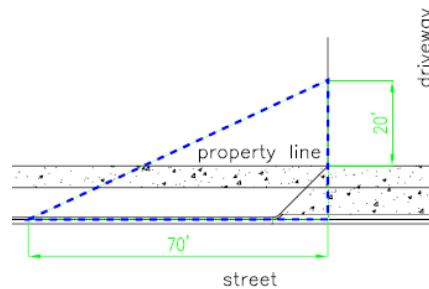
Reviewer Department/Division: FIRE

Please address the following comments:

- Fire apparatus access road is greater than 150'. An approved fire apparatus turnaround must be provided. The parking lot on the NE corner of the plan looks to be utilizing the backing stub and the three parking spaces for the turnaround. The area needs to be called out and the backing stub needs to be wider and deeper for this to work. If it is not possible to increase the area of the backing stub, then we will need to look at adding the three spaces adjacent to the current ones being used.
- The fire truck auto turn illustrated on the Fire Access plan looks like a 40' fire truck template was used. Our minimum truck length is 48.1'. I need to see how the 48.1' fire truck can negotiate the roundabout area in front of the Admin/Education Building

TECHNICAL STANDARDS:

1. City's 20'x70' sight distance triangle is not implemented correctly on landscape plan and is missing on the site plan. Show and apply the City's 20'x70' sight distance triangle both the sides of each driveway on the site plan and landscaping plan. [Sec.18-529(c) (3) CofW LDC] [Sec. 18-812 CofW LDC]. Revise at all the applicable locations and sheets as per below image. Add a note indicating that all proposed vegetation within sight triangles shall not interfere with clear visual sight lines from 30"-10'. [Sec.18-556 CofW LDC]



2. Southern driveway width is subject to variance. Please justify the hardship of not been able to provide a driveway width less than 36'. This will eliminate the requirement for the variance for maximum driveway width of 62' along the curbline.
3. Please show all the traffic control devices and pavement marking showing the traffic flow for this project.
4. The 6' sidewalk as proposed where adjacent to 90 degree parking spaces does not meet ADA requirements. An allowance for a 2.5' vehicle overhang must be considered. 4' of the sidewalk must remain clear for pedestrian use [Chapter VII, Detail SD 3-07 & SD 15-13 CofWTSSM] [Page 7-20 of CofWTSSM]. OR Provide wheel stop for these parking spaces.
5. Alternate for the above is to reduce parking length to minimum required 18' and revise sidewalk to 6.5' wide that will allow 4' clear sidewalk after 2.5' vehicle overhang.

BUS STOP:

6. Typically the bus stop shelter area should be within the public ROW. Please revise.
7. Site plan shows relocated bus stop and also existing transit stop. Clarify and label appropriate if the existing transit stop is to be discontinued.
8. Show the location of bus stop sign, type of ramp to be used with graphics for detectable warning domes in the inset provided for proposed bus stop.
9. Pedestrian crossings should be accommodated behind the departing transit vehicle. The location of bus stop shelter may need to be revised.
10. Adequate lighting should be installed around bus stops and shelters to ensure personal safety and security. Please add this comment in the note.

CROSSWALK:

11. Show all the applicable traffic regulations signs for the proposed midblock crosswalk and refuge island in the vicinity of proposed relocated bus stop south of southern driveway of this development. Refer unsignalized pedestrian crosswalk section of [MUTCD](#).
12. Revise mid-block crosswalk pavement markings to show as special emphasis crosswalk. ([CofW SD 11-11](#))

13. Pedestrian refuge detail shows variable length of island. Please clarify.
14. Dimension the width of crosswalk of the site plan.
15. Show and dimension the approach sight distances for the crosswalk. [\[AASHTO\]](#)
16. Bus stops are required to meet ADA standards. Show the landing pads and curb heights that allow for buses to load passengers in wheelchairs. This should be distinct from ADA ramp provided for mid-block crosswalk.
17. Plans are subject to further comments once we receive full traffic signage and pavement markings plan for mid-block crosswalk.

Please let me know if you have any questions or if I can be of further assistance.

Project Name: **THE HEALING PLACE OF NEW HANOVER COUNTY**

TRC Review Date: **08.13.2020**

Reviewer Name: **BILL McDow**

Reviewer Department/Division: **PDT/Transportation Planning**

TECHNICAL STANDARDS:

1. As previously stated, the proposed 37' driveway does not meet City Technical Standards for maximum driveway width at ROW. 36' is the maximum.
2. As previously stated, the proposed driveway does not meet City Technical Standards for maximum driveway width as measured at the curb and gutter line. 62' is the maximum. A variance may be requested.
3. The Auto Turns movements for the Southern Driveway, show a Fire Engine crossing the roundabout. Please show a cross section for the Roundabout and truck apron, to ensure the design will not damage the Fire Engines. The design must be mountable by City Fire Engines and the largest vehicle that will use the site.
4. As previously stated, the auto turns for the site does not show Fire Engine movements at the driveway adjacent to the Women's Building. Please revise.

[Please let me know if you have any questions regarding the comments.](#)

Project: The Healing Place
TRC Meeting Date: 8/13/20
Reviewer: Anna Reh-Gingerich
Department: Stormwater Services

To Whom It May Concern:

The Healing Place project falls within the Greenfield Lake Watershed, which currently is listed by the State for having high levels of chlorophyll. Greenfield Lake has historically had problems with severe algal blooms due to nutrient inputs from stormwater runoff. Any additional infiltration on-site would help reduce the amount of stormwater runoff and pollution that could enter Greenfield Lake and contribute to the current pollution problems.

My comments:

1. Incorporate pervious materials into sidewalks and/or the parking stalls. Any replacement of impervious material with pervious material (pavers, pervious concrete, porous asphalt) would help reduce the amount of stormwater going to the off-site treatment.
2. Incorporate bioretention/rain gardens, rain barrels, more native plantings, etc. where possible, even after construction has occurred. They can be easily incorporated into landscaping projects: [Rain Garden Fact Sheet](#); [Stormwater Solutions](#)

Thank you for the opportunity to review! Please do not hesitate to reach out to me if you have any other questions or would like to explore other ways to improve infiltration on-site.

Thank you,

Anna Reh-Gingerich

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