

To: CSD Engineering
From: Pat O’Mahony, Senior Planner; 910-341-3260
CC: File;
Date: 4/23/2024
Re: Coastal Kia Detail Building TRC Rev. 1

The following is a list of comments for review from planning regarding the project. Please provide your corrections as listed below. A staff summary of comments:

Staff	Department	Notes
Pat O’Mahony	Planning, Plan Review	Comments attached
Rob Gordon	Engineering	Comments attached
Robert Bentfield	Fire	No comments provided
Randall Glazier	Traffic Engineering	Comments attached
Bill McDow	Transportation	Comments attached

Pat O’Mahony- Planning – Attached

- Provide a dedicated landscape plan separate from site grading and utility plan.
- Show existing landscaping calculations and compliance with the table below. Transitional buffer does not apply.

Expansion (whichever is more restrictive)	Trees	Streetscape	Parking lot landscaping	Transitional buffer
10-25% increase in gross floor area or 5-10 new parking spaces	15 per disturbed acre	Half of the requirement for new construction	Half of the requirement for new construction	Half of the requirement for new construction
26-50% increase in gross floor area or 21-50 new parking spaces	15 per disturbed acre	Three-fourths of the requirement for new construction	Three-fourths of the requirement for new construction	A fence and half the requirement for new construction
51% or more increase in gross floor area, more than 20,000 square foot increase in gross floor area, or more than 50 new parking spaces	15 per disturbed acre	Full requirement for new construction	Full requirement for new construction	Full requirement for new construction

- Tree overlay detail on Sheet 2 should show proposed trees to be removed overlaid on the site improvements.
- Confirm compliance with pedestrian connections from all streets.

Rob Gordon – Engineering:

1. Note Only – P3 of the application was not filled out properly. All relevant data is included the project data table attached to the calculations. I plan to attach this table to the application.
2. Please provide a notarized signature for the application.
3. Note Only - would like to have a brief discussion in the TRC meeting about the lengthy pavement cut for the water line connection. This may trigger additional paving for acceptance.

BASE INFORMATION:

- Accurately illustrate the existing conditions on and surrounding this parcel. Show all adjacent traffic signs, lane configurations, traffic control devices, pavement markings and streetlights on the site inventory plan. Please refer to the Technical Standards and Specifications Manual (link is below) Ch. VII C.1.a.2. for specific information and distance requirements.
- Update plans to reflect current conditions/ uses including current street names.

TECHNICAL STANDARDS – ACCESS (driveway, sidewalk, and sight distance):

1. Dimension driveway widths and tapers.
2. Show and apply the City's 20'x70' sight distance triangle at each driveway and the City's 46'x46' sight distance triangle at each street corner intersection on the site plan and **landscaping plan**.
[Sec. 18-667 CofW Updated LDC] [Sec. 18-693 CofW Updated LDC].
Add a note on *site and landscaping plans* 'No parking spaces, fences, walls, posts, lights, shrubs, trees, or other type of obstructions not specifically exempted shall be permitted in the space between 30 inches and above ground and 10 feet above ground level within a triangular sight distance. [Sec. 18-667 Figure 18-667 CofW Updated LDC: Vision clearance].

TECHNICAL STANDARDS – PARKING:

3. Show all the traffic control devices and pavement marking showing the traffic flow for this project.
4. Existing parking facilities to be re-used shall meet minimum dimensional, circulation, and ingress/egress requirements of the Technical Standards and Specifications Manual and this division. [Sec. 18-344 (B) CofW Updated LDC]
5. When parking facilities are restriped, accessible parking shall comply with current Standards which includes, but not limited to accessible parking signage. (U.S. Department of Justice, Civil Rights Division, Disability Rights Section ADA Compliance Brief: Restriping Parking Spaces dated December 2015) [CofW Sign Specification and Installation Guide]

TECHNICAL STANDARDS – ADA:

6. International symbol of Accessibility parking space marking as per fig. 3B-22 of MUTCD.
7. ADA parking signage shall be outside of a 2.5' vehicle overhang area where vehicles may damage or be damaged by. [Page 7-20 of CofWTSSM]
8. Please show location of accessible ramp(s) and parking signs and provide details on the plan.
9. Note the required and proposed number of handicapped spaces in the development data. [Sec. 18-529(b)(2) CofW LDC] [Sec. 18-588 CofW Updated LDC].
10. Note/label the plan with spot elevations that clearly indicate the accessible route from the handicap space(s) to the building. [Sec. 18-529(b)(2) CofW LDC] [Sec. 18-588 CofW Updated LDC].
11. Mark ADA accessible aisle with diagonal markings.

GENERAL NOTES TO ADD TO THE PLAN:

- A. Street trees must be located a minimum of 15 feet from streetlights. [[CofW SD 15-17](#)]
- B. Any required installation or relocation of traffic signs/pavement markings is the responsibility of the project developer. Please coordinate with City Traffic Signs and pavement markings Manager/ Supervisor prior to installation/relocation of any traffic signs or markings in existing or proposed public ROW.
- C. All pavement markings in public rights-of-way and for driveways are to be thermoplastic and meet City and/or NCDOT standards. [[Detail SD 11-03 and SD 15-13 CofW Tech Stds](#)]
- D. All signs and pavement markings in areas open to public traffic are to meet MUTCD (Manual on Uniform Traffic Control Devices) standards. [[Detail SD 15-13 CofW Tech Stds](#)]
- E. All traffic control signs and markings off the right-of-way are to be maintained by the property owner in accordance with MUTCD standards.
- F. All parking stall markings and lane arrows within the parking areas shall be white.
- G. It shall be the responsibility of the subdivider to install all traffic control devices, pavement markings, and street name signs associated with the project in accordance with the Technical Standards and Specifications Manual. Contact Traffic Engineering at 341-7888 to discuss installation of traffic and street name signs. Proposed street names must be approved prior to installation of street name signs.
- H. Any broken or missing sidewalk panels and curbing will be replaced.
- I. Contact 811 prior to contacting City of Wilmington, Traffic Engineering regarding the utilities in ROW.

Please let me know if you have any questions or if I can be of further assistance.

Project Name: **COASTAL KIA DISPLAY CENTER**

TRC Date: **04.25.2024**

Reviewer Name: **BILL McDow**

Reviewer Department/Division: **PDT/Transportation Planning**

TECHNICAL STANDARDS:

1. Based upon the estimated Trip Generation numbers for the AM peak hour trips and PM Peak Hour trips, a TIA will not be required.
2. The public sidewalk does not connect to the internal sidewalk network. Please show a pedestrian path from the public sidewalk on Savings Street to the internal sidewalk network.

Project: Coastal Kia Expansion
TRC Meeting Date: 11/16/2023 ; 4/25/24
Reviewer: Anna Reh-Gingerich
Department: Stormwater

To Whom It May Concern:

The Coastal Kia Expansion project falls within the Smith Creek Watershed, which eventually drains to the Cape Fear River. Any additional infiltration or pollution treatment onsite would help reduce the amount of stormwater runoff and pollution that could enter Smith Creek and, eventually, the Cape Fear River.

My comments:

NEW

1. Thank you for incorporating native yaupon holly! Native plants require less maintenance (fertilizers, pesticides, water, etc.) than non-native plants to grow successfully since they are already acclimated to local conditions. Is there a native alternative on this list NC Cooperative Extension put together of recommended native trees, shrubs, and grasses for urban areas that could work in place of non-native trident maple?

<https://www.wilmingtonnc.gov/home/showpublisheddocument/17120/638301074568030000>

Carried over from Pre-TRC

1. Please incorporate as much tree save as possible into the site plan. Trees are helpful for improving erosion control, stormwater management, the heat island effect, air quality, and energy efficiency.
2. We encourage passive infiltration over green space or depressed bioretention areas (with curb cuts and overflows) to allow for even more infiltration and pollution treatment on the property where possible. Some examples are available at the following links:
 - a. Massachusetts "Green Parking" example: <https://www.mass.gov/service-details/demonstration-3-permeable-paving-materials-and-bioretention-in-a-parking-lot>
 - b. NCDEQ Stormwater Manual, Bioretention Cell Chapter: <https://deq.nc.gov/media/17536/download>
 - c. Filterra boxes (adding trees and stormwater management in one practice): <https://www.conteches.com/stormwater-management/biofiltration-bioretention/filterra>
 - d. ***Below are examples of bioretention, vegetated swales, and curb cuts.***

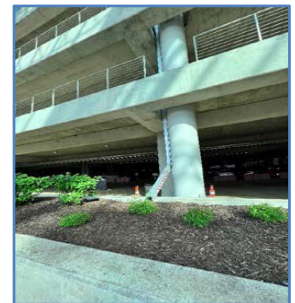


3. If the soils and water table levels allow, consider incorporating pervious materials. Any replacement of impervious material with pervious material (pavers, pervious concrete, porous asphalt, grass) would help reduce the amount of stormwater runoff being generated:

- a. <https://deq.nc.gov/media/17539/download>



4. Consider green roofs, rainwater harvesting, or redirecting some downspouts from the buildings into stormwater planter boxes (as shown on the right) to help mitigate some of the roof runoff before draining to the drainage system:
- a. Green roof <https://deq.nc.gov/media/17542/download>
 - b. RWH: <https://deq.nc.gov/media/17541/download>
 - c. https://nacto.org/docs/usdg/stormwater_planter_crwa.pdf
 - d. <https://emswcd.org/in-your-yard/rain-gardens/stormwater-planters/>



5. Properties that go above and beyond to incorporate green infrastructure are eligible to apply to the Lower Cape Fear Stewardship Development Coalition Awards: <http://www.stewardshipdev.org/>

Thank you for the opportunity to review! Please do not hesitate to reach out to me if you have any other questions or would like to explore other ways to incorporate green infrastructure into the project.

Anna Reh-Gingerich

Watershed Coordinator - Heal Our Waterways
Program

City of Wilmington Stormwater

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