

To: Richard Collier, McKim & Creed
From: Brian Chambers, Senior Planner; 910.342.2782
CC: File;
Date: 9/28/2022
Re: Center Point TRC Rev 5

The following is a list of comments for review from planning regarding the project. Please provide your corrections as listed below. A staff summary of comments:

Staff	Department	Notes
Brian Chambers	Planning, Plan Review	Comments below
Eric Seidel	Engineering	Comments attached
Chris Walker	Fire	No further comments
Mitesh Baxi	Traffic Engineering	Comments attached
Bill McDow	Transportation	Comments attached

Planning Review

Brian Chambers, brian.chambers@wilmingtonnc.gov, 910.342.2782

Comments:

- Include plaza rendering with final plan set.

Project Name: Centerpoint
Response Date: 09/16/2022
Reviewer: Eric Seidel, PE
Department: Engineering – Plan Review Section

Plans:

1. Show 50' Vegetative Buffer Setback, from Top of Bank, on Sheets CS-102 & CM-102.
2. An access easement is required from the public right-of-way to proposed 20' public drainage easement. Extend 20' public drainage easement to the public right-of-way.
3. CN-508/509: Provide weir detail for SCMs 7 & 8.

Project Name: **CENTERPOINT**

Formal TRC #5 Date: **09.20.2022**

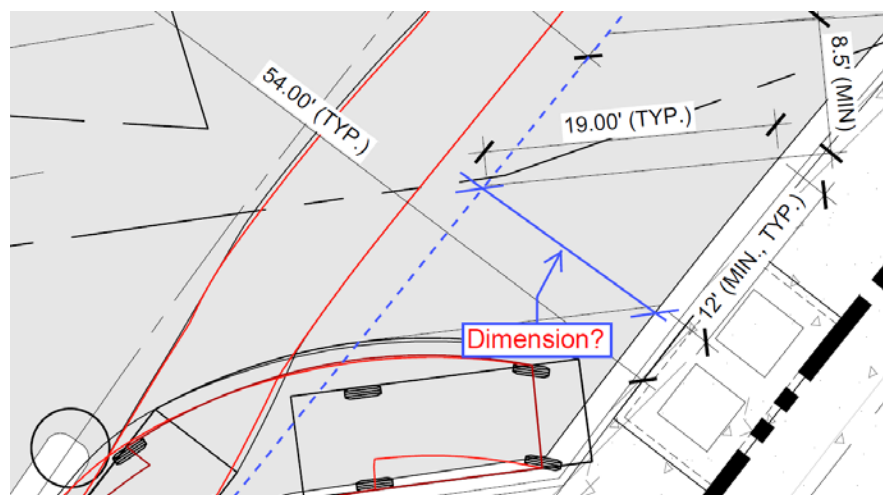
Reviewer Name: **Mitesh Baxi**

Reviewer Department/Division: **PDT/Traffic Engineering**

- Please reconcile the street names and private/public ROWs on all the sheets. Plans still shows Street A/Street B labels on sheet like CR-103, CT-100.

TECHNICAL STANDARDS:

1. City's 20'x70' sight distance triangle is not required at the street (Private/public ROW) intersections. Instead show 46'x46'.
2. Clarify the requirement of 30'x70' sight distance triangle shown on sheet CR-101.
3. 46'x46' SDT are still called out at the roundabout on sheet CS-101.
4. Dimension the angle of angled parking spaces on site plan as per [Chapter VII, Table 6, page 7-19 CofW TSSM].
5. Angled parking spaces dimensions must meet minimum requirements for parking from City Technical Standard, [Chapter VII, Table 6, page 7-19 CofW TSSM]. 19' minimum is measured from rear to front of stall perpendicularly to curb for 45-degree spaces. A waiver at SRB was to allow angled on-street parking subject to be built as per City technical standards. Auto turn shows insufficient length proposed. Look for blue markups in an image below.



STREET LIGHTING [City of Wilmington Street Lighting Policy]:

- All the ROWs for this project are now proposed as private ROW in latest version except Calypso Dr extension.
- The streetlights installed on private ROWs are not eligible for reimbursement from City and not eligible to get transferred to City's account.
- Streetlights installed on private ROWs shall be maintained by HOA/PME of the property.
- No further review of the streetlighting plan is required for private streets.
- Reconcile the location of streetlights for Calypso Dr on lighting plan and landscape plan.
- Please add the note as per below within City of Wilmington notes on Site plan.
'Streetlights are required to be installed by the responsible authority of this project along abutting streets of Eastwood Rd, Drysdale Dr extension and Military Cutoff Rd as a part of this development. This should be coordinated with NCDOT's future project at Eastwood Rd and Military Cutoff Rd intersection improvement. Contact Duke Energy to get the plans developed in compliance with AASHTO standards.'

ROUNDBOUT 1 (Swain Way/Center Point Dr):

- Provide stopping sight distance on approach, stopping sight distance on circulatory roadway (across the island), intersection, and to crosswalk on exit with dimensions. Refer NCHRP Report 672 Roundabouts An Informational Guide and revise all the sight distances accordingly.
- Detectable warning domes at the refuge island if crosswalk length at cut-through is sufficient (at least 6'). Do not show or callout detectable warning domes if less than 6'.
- Flare should be provided at the splitter island nose to be consistent with entry width at yield line and delineate traffic appropriately.
- Dimension the distance of crosswalk from entrance lines for all the legs. This distance should be approximately in increments of vehicle lengths.
- Plan shows fire auto turns for this roundabout. Provide fire auto turns for any other locations, if expected to maneuver.

ROUNDBOUT 2 (Calypso Dr/Cavalier Dr):

- Sheet CR-501 – Plan shows fully traversable truck apron. Where provided, truck aprons should be designed with a curbed edge high enough to discourage passenger vehicles from traversing over the top of the apron. If truck apron is not provided central island diameter can be increased to delineate traffic appropriately.
- Provide auto-turns. Consider largest vehicles maneuvering for LIDL store access.

SIGNAGE AND PAVEMENT MARKINGS:

- If R1-5a yield here to pedestrian sign is not proposed, remove the details from plans.
- Military Cutoff Rd and Eastwood Rd are NCDOT streets. The street name signs at these intersections shall be installed on the separate stand-alone pole in advance of the stop sign.
- Show and callout skip lines for taper and left turn lane at the eastern leg of roundabout off Street A and Street B.
- Proposal of roundabout at Calypso Dr/ Cavalier Dr will change the lane configuration of Cavalier Dr section south bound till Eastwood Rd. Show the full pavement markings with all the relevant details for these changes.

Please let me know if you have any questions or if I can be of further assistance.

PROJECT NAME: CENTER POINT TRC PLAN

TRC DATE: 09.27.2022

REVIEWER NAME: Bill McDow

REVIEWER DEPARTMENT/DIVISION: PDT/ Transportation Planning

TECHNICAL STANDARDS – NEW ROADS:

1. The proposed Stop Bars and Stop Signs at the intersections along Center Point Drive, are over 25' from the intersection. Please move the cross walks, Stop Bars and Stop Signs closer to the travel lane on Center Point Drive.
2. The Auto -Turns movements for the Fire Engine appear to show the wheel path of the vehicle running over the ends of the splitter islands and central island for the mini-roundabout Please consider increasing the travel lanes from 14' to 16' for the roundabout or making the splitter islands and center island mountable.
3. Street trees are located within the 46'X46' Sight Distance Triangles. Please move the trees from the sight distance triangles or trim the vegetation/ limbs from the area of 30" to 10' for trees within the sight distance triangles.
4. No Further Comments.