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DATE: 12.12.2012

TO: ProTrack

FROM: Tara S. Murphy, ASLA/RLA/LEED AP Dave Brent
 Transportation Planning Traffic Engineering

■ **BULLZEYE SHOOTING SPORTS [Initial Review]** Per the Planning Department, this is considered a change in use and is reviewed accordingly.

🌀 Initial Review Note 🌀

All comments are based on adopted policy documents, specific City of Wilmington Land Development Code (CofW LDC) and City of Wilmington Technical Standards and Specifications Manual (CofWTSSM) requirements in effect at the time of this review. These have been noted after the comment as appropriate for your ease in further research.

The City Engineer may administratively approve dimensional variance requests per LDC Sec. 18-348. If the Applicant feels further interpretation is in order, please contact Transportation Planning directly to discuss in additional detail. Please apply for all administrative variances in writing, an email is acceptable or this may be included on the plan set. Please see Sec. 18-348 of the Land Development Code for the information required to process a variance request.



NCDOT:

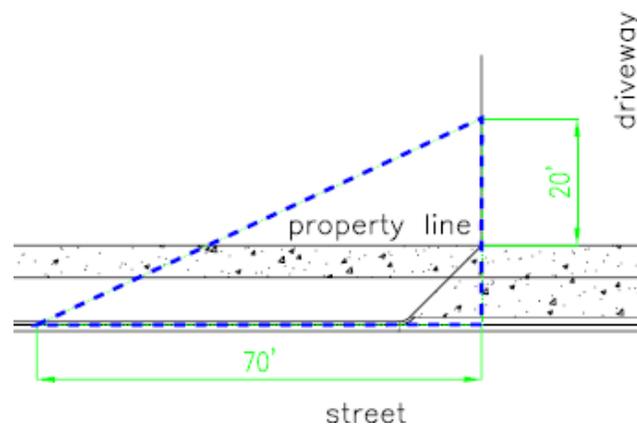
Market Street/ US HWY 17 is a North Carolina State Department of Transportation facility. It is the Applicant's responsibility to coordinate directly with NCDOT to determine if any driveway permits/ revisions/ review processes are required. Contact David Leonard, PE at 910.251.2655. Please note additional comments may be forthcoming once coordination occurs.

TECHNICAL STANDARDS:

1. The driveway shall be City-standard ramp-type driveway. Show appropriate City standard detail on plan. [\[Chapter VII , CofWTSSM\]](#)
2. Show proposed driveway centerline elevations at both the front and the back of each concrete driveway apron, at the curb/edge of pavement line, property line & points at 26' and 52' behind the property line. [\[Chapter VII, C\(1\)\(a\)\(2\)12 CofWTSSM\]](#)
3. Dimension driveway widths and tapers. [\[Sec.18-529 CofW LDC\]](#)
4. Provide a sidewalk connection between the site and the public sidewalk.
5. Provide sidewalk detail SD 8-15 on the plan.
6. Dimension the western bay of parking stalls, all radii and parking aisles. [\[Sec. 18-529\(b\)\(2\) CofW LDC\]](#)
7. The minimum radius is 25' for any portion of a parking lot adjacent to a travel way (i.e. islands at the end of a parking bay) for parking that is open to the public. If the travel way will not be used for emergency service vehicles or truck traffic, you may request a tighter radius, the minimum is 15'. This

request must be made in writing (an email message is acceptable). [\[Chapter VII, Detail SD 15-13 CofWTSSM\]](#)

8. A turnaround for emergency vehicles is required due to the distance to the rear of the site (greater than 150'). Radii for the turnaround shall be a minimum 25'.
9. Provide additional dimensions to clearly indicate the drive aisle widths at areas where proposed improvements alter the width, such as by the existing building.
10. It appears based on the calculations in the site data table that the existing building is to remain? Please confirm by labeling the intent on the site plan.
11. Provide a turning movement analysis of trash truck access to the dumpster area.
12. Although not a requirement, it is requested the Applicant consider adding some bicycle parking.
13. Please show location of handicap ramp (if any) and signs and provide details on the plan.
14. Note/label the plan with spot elevations that clearly indicate the accessible route from the HC space(s) to the building. Provide striping as needed to indicate the accessible route from the parking space to the building entrance. [\[Sec. 18-529\(b\)\(2\) CofW LDC\]](#)
15. Note the required and proposed number of handicapped spaces in the development data. [\[Sec. 18-529\(b\)\(2\) CofW LDC\]](#)
16. Show and apply the City's 20'x70' sight distance triangle on the site plan and landscaping plan. [\[Sec.18-529\(c\)\(3\)CofW LDC\]](#)



MISCELLANEOUS:

- ❖ Permitting of business identification signage is a separate process. NCDOT/ City of Wilmington will not allow obstructions within the right of way.
- ❖ Transportation Planning Staff will review the landscape plan once submitted for compliance with sight distance requirements.

Please let me know if you have any questions or if I can be of further assistance as this development moves through the review process.