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DATE: 11.05.2013

TO: ProTrak

FROM: Bill McDow
 Transportation Planning

Dave Brent
 Traffic Engineering

■ **PANERA BREAD [Plan review #2]**



BASE INFORMATION:

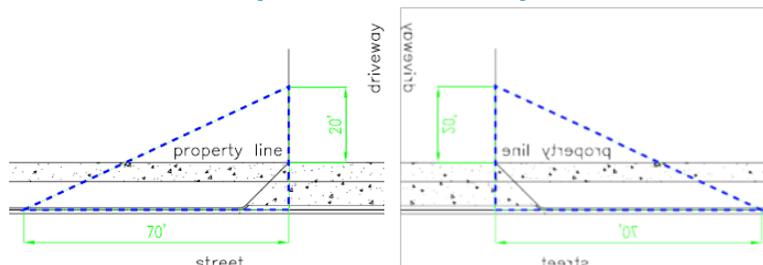
- As previously mentioned, pavement markings on site (SW parking lot) are weathered and may not in be in accordance with the MUTCD standard of maintenance. Additionally, the curb line for this parking area must be adjusted to allow room for the future right turn lane along Independence Blvd. Show the markings as proposed on the site plan.

TRAFFIC IMPACT:

- ❖ Per the 08142013 approval of the TIA waiver letter, it was requested that the applicant ensure that site elements such as parking be designed to allow for the future installation of a northbound right turn lane on Independence Blvd. at Oleander Drive Intersection (by others). Therefore, it is requested that site elements required in association with the Panera development be located/relocated so as not to negatively impact the ability for others to construct the identified (right turn lane) improvement. This improvement will impact the SW parking lot for the site.

TECHNICAL STANDARDS – ACCESS (driveway, sidewalk, sight distance, traffic signal facilities, and street lights):

1. Thank you for providing a note regarding coordination with Traffic Engineering on the proposed work on the 8" sewer line and adjacent Traffic Signal facilities/ infrastructure.
2. The Sight Distance triangles were not correctly shown on the site plan. The triangle must be applied on both sides of the driveway. (See illustration below). Show and apply the City's 20'x70' sight distance triangle at each driveway on the site plan and landscaping plan. [\[Sec.18-529\(c\)\(3\)CofW LDC\]](#) Add a note indicating that all proposed vegetation within sight triangles shall not interfere with clear visual sight lines from 30"-10'. [\[Sec.18-566 CofW LDC\]](#)



TECHNICAL STANDARDS – PARKING:

3. Wheel stops shall not exceed 4" in height. The detail shown on sheet C-06 has a 6" high wheel stop.

TECHNICAL STANDARDS – Barrier Free Design:

4. The tactile mats seemed to be missing on this version of the site plan. They were present in the notes and the detail sections; however, I did not see them on sheet C-02. Therefore, show the tactile mats for accessible curb ramps and wheel chair ramps on the site and at sidewalk connections.

Please let me know if you have any questions or if I can be of further assistance as this development moves through the review process.