



**Planning, Development  
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**DATE:** 10.31.2017  
**TO:** ProTrak  
**FROM:** Mitesh Baxi  
Traffic Engineering

■ **PEACE BAPTIST CHURCH [TRC Plan Review]**

☞ Initial Review Note ☞

All comments are based on adopted policy documents, specific City of Wilmington Land Development Code (CofW LDC) and City of Wilmington Technical Standards and Specifications Manual (CofWTSSM) requirements in effect at the time of this review. These have been noted after the comment as appropriate for your ease in further research.

The City Engineer may administratively approve dimensional variance requests per LDC Sec. 18-348. If the Applicant feels further interpretation is in order, please contact Transportation Planning directly to discuss in additional detail. Please apply for all administrative variances in writing, an email is acceptable or this may be included on the plan set. Please see Sec. 18-348 of the Land Development Code for the information required to process a variance request.



**BASE INFORMATION:**

- The site is within 500’ of a Traffic Signals. Revise all sheets to accurately illustrate the existing conditions on and surrounding this parcel. Show all adjacent traffic signs, lane configurations, traffic control devices and pavement markings for the driveway intersection with Military Cutoff Road. Additionally, also include these details for intersection of Military Cutoff Road & Arboretum Drive on the site inventory plan. Please refer to the Technical Standards and Specifications Manual (link is below) Ch. VII C.1.a.2.for specific information and distance requirements. Contact Traffic Engineering at 341-7888 if you need assistance locating these elements.
- If pavement markings on site have weathered to the point they are not in accordance with the MUTCD standard of maintenance, must be maintained by the developer accordingly.

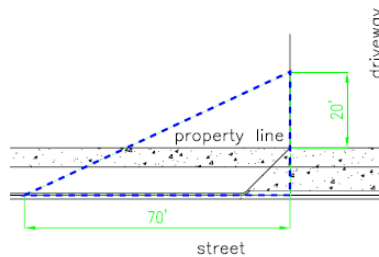
**NCDOT:**

It is the Applicant’s responsibility to coordinate directly with NCDOT to determine if any driveway permits/ revisions/ review processes are required. Contact NCDOT at 910. 251.2655. Please note additional comments may be forthcoming once coordination occurs.

**TECHNICAL STANDARDS – ACCESS (driveway, sidewalk, and sight distance):**

1. Driveways shall be City-standard ramp-type driveways. Show City standard detail(s) on plan. [\[SD 3-03.3 & 3-03.4 \(Vertical curb\) CofWTSSM\]](#). Where no curb exists in the vicinity, a flat slab type or a street type entrance may be allowed or required by the City Engineer. [\[page 7-9 CofWTSSM\]](#)
2. Dimension driveway widths and tapers. [\[Sec.18-529 CofW LDC\]](#)

3. A pavement marking plan is required for all driveways greater than 30' in width. [page 7-9 CofWTSSM]
4. Label the proposed and existing sidewalk(s) appropriately and provide dimensions.
5. Provide sidewalk detail SD 3-10 on the plan.
6. Show all adjacent traffic signs and pavement markings on the plan. [Sec. 18-529(b)(2) CofW LDC]
7. Show any traffic signal facilities and equipment in front of the development. Contact Traffic Engineering at 341-7888 for help in identifying these.
8. Although site plan shows 10'x70' NCDOT sight distance triangle, additionally show and apply the City's 20'x70' sight distance triangle at the on the site plan and landscaping plan. [Sec.18-529(c)(3)CofW LDC] Add a note indicating that all proposed vegetation within sight triangles shall not interfere with clear visual sight lines from 30"-10'. [Sec.18-556 CofW LDC]



#### TECHNICAL STANDARDS – PARKING:

9. Distinguish between existing and proposed parking stalls. It is not clear whether all the parking stalls or only part of it are to be redesigned. You may specify the existing and proposed number of parking spaces in the site data.
10. All off street parking areas shall be arranged so that ingress and egress is by forward motion of vehicles. [Sec.18-526 CofW LDC]
11. Dimension parking stalls, radii and parking aisles. [Sec. 18-529(b)(2) CofW LDC]
12. The minimum standard vehicle parking space size is 8.5' in width by 18' in length.
13. The minimum radius is 25' for any portion of a parking lot adjacent to a travel way (i.e. islands at the end of a parking bay) for parking that is open to the public. If the travel way will not be used for emergency service vehicles or truck traffic, you may request a tighter radius, the minimum is 15'. This request must be made in writing (an email message is acceptable). [Chapter VII, Detail SD 15-13 CofWTSSM]
14. Twenty-four feet (24') is the minimum drive aisle width behind perpendicular parking.
15. Protection from vehicles is required around all required landscaped areas within vehicular areas.
16. Parking lot must meet requirements of standard details SD 15-12 and 15-13 for parking lots equal to or greater than 25 stalls.
17. Provide bicycle parking in accordance with Section 18-528 of the City of Wilmington Land development Code. Please add the required and the proposed number to the site data table, and graphically indicate the location of bike parking on the plans. [Sec.18-528 (f) CofW LDC]

#### TECHNICAL STANDARDS – Barrier Free Design:

18. Show the typical handicap sign as per ADA and City standards. [Sheets A1 of 5 and A2 of 5, CofW Sign Specification]
19. Boundary of accessible aisle must be marked closed, to discourage the parking in it.
20. Please show location of handicap ramps and signs and provide details on the plan.
21. Note/label the plan with spot elevations that clearly indicate the accessible route from the HC space(s) to the building. [Sec. 18-529(b)(2) CofW LDC]
22. At least one accessible aisle, located towards the driveway entrance, is not connected to the accessible route. Please extend the accessible route or relocate this accessible parking.

**GENERAL NOTES TO ADD TO THE PLAN:**

A. All parking stall markings and lane arrows within the parking areas shall be white.

**MISCELLANEOUS:**

- ❖ We will reserve comments on the parking lot, driveway geometry and other technical issues until a full construction drawing is submitted.

Please let me know if you have any questions or if I can be of further assistance as this development moves through the review process.

Bicycle parking. Each new multifamily, commercial, or office development or major redevelopment requiring twenty-five (25) or more automobile parking spaces shall make provisions for parking a minimum of five (5) bicycles. Each additional one hundred (100) automobile parking spaces above the twenty-five (25) minimum shall require provisions for parking an additional five (5) bicycles up to a bicycle parking system that can accommodate a maximum of twenty (20) bicycles. Bicycle parking facilities shall be provided within two hundred (200) feet of the primary entrance to the facility. In the event of multiple entrances, bicycle-parking facilities shall be dispersed for easy access to the multiple entrances.