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**DATE:** 010.28.2013

**TO:** ProTrak

**FROM:** Bill McDow  
 Transportation Planning

Dave Brent  
 Traffic Engineering

■ **MAYFAIRE COMMUNITY CENTER PHASE II [plan review #2]**

🌀 Initial Review Note 🌀

All comments are based on adopted policy documents, specific City of Wilmington Land Development Code (CofW LDC) and City of Wilmington Technical Standards and Specifications Manual (CofWTSSM) requirements in effect at the time of this review. These have been noted after the comment as appropriate for your ease in further research.

The City Engineer may administratively approve dimensional variance requests per LDC Sec. 18-348. If the Applicant feels further interpretation is in order, please contact Transportation Planning directly to discuss in additional detail. Please apply for all administrative variances in writing, an email is acceptable or this may be included on the plan set. Please see Sec. 18-348 of the Land Development Code for the information required to process a variance request.

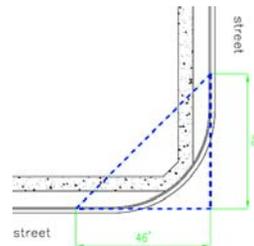
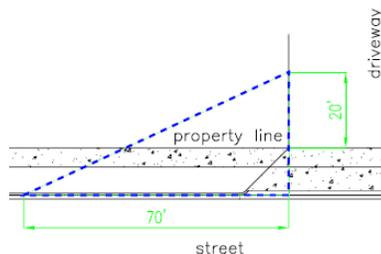


**TRAFFIC IMPACT:**

- ❖ Thank you for providing the update on the Trip Generation Data for the Master Plan for the Mayfaire Development. If the Master Plan Trip Generation Data is correct, then an update to the TIA will not be required.

**TECHNICAL STANDARDS – ACCESS (driveway, sidewalk, sight distance):**

1. The landscaping plan does not have sight distance triangles. Show and apply the City’s 20’x70’ sight distance triangle at each driveway and the City’s 46’x46’ sight distance triangle at each street corner intersection on the landscaping plan. [\[Sec.18-529\(c\)\(3\)CofW LDC\]](#)



**TECHNICAL STANDARDS – PARKING:**

2. As previously stated, the handicap parking (parking aisle and crosswalk closest to Bldg C) adjacent to the curve in the main drive aisle east of the building needs to be revised. A vehicle backing out of the

parking space should not be backing into 2 different drive/parking aisles. This intersection also causes a sight distance issue for vehicles heading westbound. Adjusting the island radii will likely solve this issue

3. The Auto turns analysis provided was for a WB 65 vehicle, instead of a WB-50 vehicle. Please clarify the largest type of vehicle that will be providing deliveries/ accessing the site.

**TECHNICAL STANDARDS – Barrier Free Design:**

4. Provide the necessary clearance between vehicles and the location of handicap signs on the site plan. An allowance for a 2.5' vehicle overhang must be considered. 4' of the sidewalk must remain clear for pedestrian use. [\[Chapter VII, Detail SD 15-13 CofWTSSM\]](#)
5. Increase the handicap ramp landing area/ clearance area for the 5' sidewalk located at the northern corner of Building C.

**REVISIONS TO NOTES ON THE PLAN:**

6. Please revise Contractors notes # 3 on sheet C3.2 to reflect the following verbiage:  
#3 A minimum clearance of 2.5" shall be maintained between the face of curb and any part of a traffic sign or light pole.

**MISCELLANEOUS:**

- ❖ Include appropriate City of Wilmington approval stamps prior to submitting final plans.

Please let me know if you have any questions or if I can be of further assistance as this development moves through the review process.