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DATE: 09.04.2014
TO: ProTrak
FROM: Bill McDow
 Transportation Planning

■ **Realigned River Road [TRC Plan Review]**

↻ Initial Review Note ↻

All comments are based on adopted policy documents, specific City of Wilmington Land Development Code (CofW LDC) and City of Wilmington Technical Standards and Specifications Manual (CofWTSSM) requirements in effect at the time of this review. These have been noted after the comment as appropriate for your ease in further research.

The City Engineer may administratively approve dimensional variance requests per LDC Sec. 18-348. If the Applicant feels further interpretation is in order, please contact Transportation Planning directly to discuss in additional detail. Please apply for all administrative variances in writing, an email is acceptable or this may be included on the plan set. Please see Sec. 18-348 of the Land Development Code for the information required to process a variance request.

BASE INFORMATION:

- Modify street cross section for River Road at Independence Blvd to reflect current width of River Road. The roadway does not reduce to 28’ until after the CFPWA pump station site NHC-76 beyond Barnard’s Creek Bridge.
- Show existing conditions on River Road including Northbound Left Turn Lane, South Bound Left Turn Lane, Bike Lanes and pavement markings on the plan.
- Update all sheets to reflect the changes to the roadway, force main, water and sewer as a result of City, NCDOT and CFPWA projects conducted between 2007 and the present. These changes include the 20” Force Main and Interim Pump Station upgrades initiated by the CFPWA and referenced in the Development Agreement Amendment 3. C. Water and Sewer dated March 18, 2013.

TRAFFIC IMPACT:

- ❖ A Traffic Impact Analysis (TIA) was performed for the River Lights project with a 2018 full build out date and a 2013 phase 1 build out date. As the phase 1 building date has passed, it is necessary to publish an updated schedule and phasing for the project. Please contact Amy Kimes, PE at 910-473-5130 to discuss the schedule and phasing for the project.

NCDOT:

- As the existing River Road has been abandoned by NCDOT for maintenance to the COW, applicant will provide the records for this roadway to the city.
- As NCDOT has provided specific approvals to the applicant for the new sections of River Road, the applicant will provide copies of any agreements received by NCDOT prior to this submittal.
- Please provide any NCDOT encroachment agreements, driveway permits or roadway or bridge approvals for the proposed River Road Realignment.

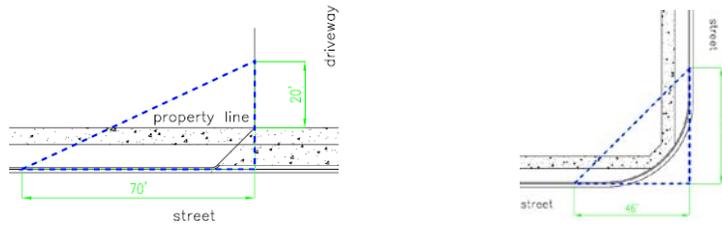
- The River Road realignment section will be on City Streets; therefore, city curb and gutter sections will be required where applicable.
For any streets that remain under NCDOT maintenance, applicant will contact NCDOT to coordinate any driveway permits/ revisions/ review processes that are required. Contact Allen Hancock, PE at 910. 251.2655. Please note additional comments may be forthcoming once coordination occurs.

TECHNICAL STANDARDS – NEW ROADS:

1. The minimum standard median curb is 24" vertical curb. If the applicant is proposing another type curb for the median sections, a variance request must be made.
2. Verify location of proposed utilities within the street cross section and alignment, to include fire hydrant space over 500' and location of manholes adjacent to splitter and traffic islands.
3. The proposed cross section shall be revised to show the minimum base layer on a collector street and major thoroughfare to 6" and extension of the base layer 1' beyond the curb. [\[Chapter II, Sec. F \(4\) CofWTSSM\]](#)
4. Provide adequate vehicle deceleration lanes and taper for vehicles at the proposed right and left turn lanes. As River Road is a 55 mph facility, 200' will not provide a safe and efficient deceleration lane for this project.
5. Modify all driveway storage lengths to reflect requirements of the River Road Development TIA improvements. For example, Driveway 1 Eastbound approach requires a minimum of 200' full storage, however, the detail for this driveway only provides ~100' of storage.
6. For roundabout 1 and the northbound departure lane, please provide the required 1200' of departure lane specified in the TIA. Failure to provide adequate departure lanes and tapers will result in poor facility level of service and increase traffic congestion and delay on River Road and the adjacent road network.
7. For the proposed Roundabouts, please provide the location where bicycles will leave the roadway and bypass the center of the roundabout. If the cyclists are to remain within the roundabout, lane widths and pavement markings will have to be modified to show the location of cyclists within the roundabout.
8. Provide turning movements for WB-50 vehicles along the primary intersections on River Road to include the Roundabouts, Marina Village Intersection, Lorraine Dr, and Wilderness Road intersections.
9. Install wheelchair ramp at the corners of the project, per NCDOT and/or City standards.

TECHNICAL STANDARDS – ACCESS (driveway, sidewalk, sight distance):

10. Provide a plan for vertical curb and/or horizontal separation required to provide clearance between the at-grade sidewalk and vehicular traffic. As this facility is a 55 mph facility, some means of protection shall be provided.
11. Provide additional details for the pedestrian tunnel reference on sheet CU-706.
12. Show the location of the 10' multi-use Path on the site plan and show points of access from the trail to the public sidewalk and intersections on River Road.
13. Distinguish between proposed and existing sidewalk(s) and provide dimensions
14. Show all adjacent traffic signs and pavement markings on the plan. [\[Sec. 18-529\(b\)\(2\) CofW LDC\]](#)
15. Show any traffic signal facilities and equipment in front of the development. Contact Traffic Engineering at 341-7888 for help in identifying these.
16. Show and apply the City's 20'x70' sight distance triangle at each driveway and the City's 46'x46' sight distance triangle at each street corner intersection on the site plan and landscaping plan. [\[Sec.18-529\(c\)\(3\)CofW LDC\]](#) Add a note indicating that all proposed vegetation within sight triangles shall not interfere with clear visual sight lines from 30"-10'. [\[Sec.18-566 CofW LDC\]](#)



TECHNICAL STANDARDS – Barrier Free Design:

17. Please show location of handicap ramp(s), tactile mats and signs and provide details on the plan.
18. Provide the location and details for any crosswalks on the site plan, including the high visibility crosswalks proposed for the roundabouts on the site plan.
19. The refuge islands for the roundabout at Lorraine Drive do not appear to be adequate to provide a minimum of 1 vehicle storage between the travel lane of the roundabout and the crosswalk. Vehicles waiting to enter the travel lane will block the crosswalk if this storage length is not provided for the splitter islands.

GENERAL NOTES TO ADD TO THE PLAN:

- A. All pavement markings in public rights-of-way and for driveways are to be thermoplastic and meet City and/or NCDOT standards. [\[Detail SD-13 CofW Tech Stds\]](#)
- B. All signs and pavement markings in areas open to public traffic are to meet MUTCD (Manual on Uniform Traffic Control Devices) standards. [\[Detail SD 15-13 CofW Tech Stds\]](#)
- C. All traffic control signs and markings off the right-of-way are to be maintained by the property owner in accordance with MUTCD standards.
- D. A utility cut permit is required for each open cut of a City street. Note this on the plan and contact 341-5888 for more details. In certain cases an entire resurfacing of the area being open cut may be required.
- E. Install reflectors per City and NCDOT standards. Traffic Engineering must approve of pavement marking layout prior to actual striping.
- F. It shall be the responsibility of the subdivider to erect official street name signs at all intersections associated with the subdivision in accordance with the Technical Standards and Specifications Manual. The subdivider may acquire and erect official street name signs or may choose to contract with the city to install the street signs and the subdivider shall pay the cost of such installation. Contact Traffic Engineering at 341-7888 to discuss installation of traffic and street name signs. Proposed street names must be approved prior to installation of street name signs.
- G. Any broken or missing sidewalk panels, driveway panels and curbing will be replaced.
- H. A landscaping plan indicating the location of required street trees shall be submitted to the City of Wilmington Traffic Engineering Division and Parks and Recreation Department for review and approval prior to the recording of the final plat. [\[SD 15-14 CofW Tech Stds\]](#)
- I. Contact Traffic Engineering at 341-7888 forty-eight hours prior to any excavation in the right of way.

MISCELLANEOUS:

- ❖ Include appropriate City of Wilmington approval stamps prior to submitting final plans.
- ❖ Transportation Planning Staff reserves the right to review and comment on any additional plans submitted for this development.

Please let me know if you have any questions or if I can be of further assistance as this development moves through the review process.