



**Planning, Development
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■ **MAYFAIRE SELF DEVELOPMENT [TRC Plan Review]**

↻ Initial Review Note ↻

All comments are based on adopted policy documents, specific City of Wilmington Land Development Code (CofW LDC) and City of Wilmington Technical Standards and Specifications Manual (CofWTSSM) requirements in effect at the time of this review. These have been noted after the comment as appropriate for your ease in further research.

The City Engineer may administratively approve dimensional variance requests per LDC Sec. 18-348. If the Applicant feels further interpretation is in order, please contact Transportation Planning directly to discuss in additional detail. Please apply for all administrative variances in writing, an email is acceptable or this may be included on the plan set. Please see Sec. 18-348 of the Land Development Code for the information required to process a variance request.

BASE INFORMATION:

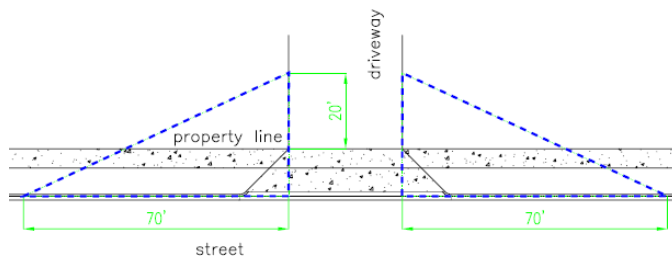
- The site plans do not show the parking spaces assigned for these restaurants. 90 parking spaces were listed in the site data table, however, most of the parking spaces are not shown on the site plan.

TRAFFIC IMPACT:

- ❖ Thank you for submitting the Mayfaire Town Center Trip Generation Comparison. A Traffic Impact Analysis (TIA) will not be required for this development if the above information is consistent with the proposed use. Contact Transportation Planning prior to resubmitting plans if the intended use of the site is different than what is outlined in this letter.

TECHNICAL STANDARDS – ACCESS (driveway, sidewalk, and sight distance):

1. The site appears to have two driveways along International Drive. Dimension driveway widths and tapers. [Sec.18-529 CofW LDC]
2. The minimum two-lane driveway width is 23'. [7-9 CofW Tech Stds]
3. The maximum two-lane driveway width is 30'. [7-9 CofW Tech Stds]
4. Show and apply the City's 20'x70' sight distance triangle at each driveway and the City's 46'x46' sight distance triangle at each street corner intersection on the site plan and landscaping plan. [Sec.18-529(c)(3)CofW LDC] Add a note indicating that all proposed vegetation within sight triangles shall not interfere with clear visual sight lines from 30"-10'. [Sec.18-566 CofW LDC]



TECHNICAL STANDARDS – PARKING:

5. The minimum radius is 25' for any portion of a parking lot adjacent to a travel way (i.e. islands at the end of a parking bay) for parking that is open to the public. If the travel way will not be used for emergency service vehicles or truck traffic, you may request a tighter radius, the minimum is 15'. This request must be made in writing (an email message is acceptable). [[Chapter VII, Detail SD 15-13 CofWTSSM](#)]
6. Twenty-four feet (24') is the minimum drive aisle width behind perpendicular parking.
7. The site data table does not show the number of required and provided bicycle parking in accordance with Section 18-528 of the City of Wilmington Land development Code. Add the number required and proposed to the site development data. [[Sec.18-528 CofW LDC](#)]

TECHNICAL STANDARDS – Barrier Free Design:

8. The location of the Van Accessible Sub-plate in Site Details 04 does not meet the MUTCD Placement of Van Accessible Plaque (R7-8P) with the Restricted Parking Sign (R7-8) **MUTCD Standard:**

08 Where parking spaces that are reserved for persons with disabilities are designated to accommodate wheelchair vans, a VAN ACCESSIBLE (R7-8P) plaque shall be mounted below the R7-8 sign. The R7-8 sign (see Figure 2B-24) shall have a green legend and border and a white wheelchair symbol on a blue square, all on a white background. The R7-8P plaque (see Figure 2B-24) shall have a green legend and border on a white background.

MISCELLANEOUS:

- ❖ Permitting of business identification signage is a separate process. NCDOT/ City of Wilmington will not allow obstructions within the right of way.
- ❖ Transportation Planning Staff will review the landscape plan once submitted for compliance with sight distance requirements.

Please let me know if you have any questions or if I can be of further assistance as this development moves through the review process.