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DATE: 03.13.2014
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■ **HAN-DEE HUGO [Initial Plan Review]**

↻ Initial Review Note ↻

All comments are based on adopted policy documents, specific City of Wilmington Land Development Code (CofW LDC) and City of Wilmington Technical Standards and Specifications Manual (CofWTSSM) requirements in effect at the time of this review. These have been noted after the comment as appropriate for your ease in further research.

The City Engineer may administratively approve dimensional variance requests per LDC Sec. 18-348. If the Applicant feels further interpretation is in order, please contact Transportation Planning directly to discuss in additional detail. Please apply for all administrative variances in writing, an email is acceptable or this may be included on the plan set. Please see Sec. 18-348 of the Land Development Code for the information required to process a variance request.



BASE INFORMATION:

- Revise all sheets to accurately illustrate the existing conditions on and surrounding this parcel. Show all adjacent traffic signs, lane configurations, traffic control devices and pavement markings on the site inventory plan. Please refer to the Technical Standards and Specifications Manual (link is below) Ch. VII C.1.a.2. for specific information and distance requirements. Contact Traffic Engineering at 341-7888 if you need assistance locating these elements.
- If pavement markings on site have weathered to the point they are not in accordance with the MUTCD standard of maintenance, do not show the markings as existing, rather show them as proposed on the site plan.
- A zoning line is shown on the site plan, please verify the location of this line and label all zoning and easements on this site.

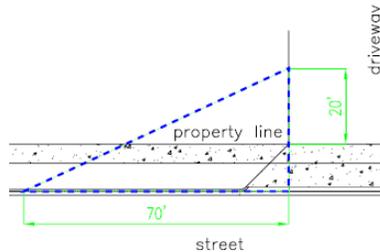
TRAFFIC IMPACT:

- ❖ The site has a net increase of 1086 SF. A Traffic Impact Analysis (TIA) is not required for this development based on the net increase in estimated trips generated by this use.
- ❖ A Traffic Impact Analysis (TIA) will not be required for this development if the above information is consistent with the proposed use. Contact Transportation Planning prior to resubmitting plans if the intended use of the site is different than what is outlined above.

TECHNICAL STANDARDS – ACCESS (driveway, sidewalk, sight distance):

1. Show driveways for adjacent lots and lots across the street.
2. Distinguish between proposed and existing sidewalk(s) and provide dimensions.
3. Provide sidewalk detail SD 3-10 on the plan.

4. The site plans refer to Sheet P-200 (Utility Plan sheet # C103); however, it was not included.
5. Show all adjacent traffic signs and pavement markings on the plan. [Sec. 18-529(b)(2) CofW LDC]
6. Show and apply the City's 20'x70' sight distance triangle at each driveway on the site plan and landscaping plan. [Sec.18-529(c)(3)CofW LDC] Add a note indicating that all proposed vegetation within sight triangles shall not interfere with clear visual sight lines from 30"-10'. [Sec.18-566 CofW LDC]



TECHNICAL STANDARDS – PARKING:

7. The minimum radius is 25' for any portion of a parking lot adjacent to a travel way (i.e. islands at the end of a parking bay) for parking that is open to the public. If the travel way will not be used for emergency service vehicles or truck traffic, you may request a tighter radius, the minimum is 15'. This request must be made in writing (an email message is acceptable). [Chapter VII, Detail SD 15-13 CofWTSSM]
8. Protection from vehicles is required around all required landscaped areas within vehicular areas.
9. As the developer has chosen to provide automobile spaces, bicycle parking shall be provided in accordance with Sec. 18-528. Please add the required number and the proposed number to the site data table and graphically indicate on the plans the location of bike parking. [Sec.18-528 CofW LDC]

TECHNICAL STANDARDS – Barrier Free Design:

10. Provide wheel stops for parking spaces perpendicular to any proposed retaining walls to allow for car overhang and prevention of cars hitting the wall.
11. The proposed sidewalk adjacent to 90 degree parking spaces shall be a minimum 5' in order to meet ADA requirements. An allowance for a 2.5' vehicle overhang must be considered. 4' of the sidewalk must remain clear for pedestrian use. [Chapter VII, Detail SD 15-13 CofWTSSM]

REVISIONS TO NOTES ON THE PLAN:

12. Please revise spelling within notes # 2 (**protected**); #23 (**contractor**); #25 (**contact, Fire and Life Safety**); #26 (**obstructions**); #27 (**contact and clearing**) on sheet C101 and "30666 square **feet**" within Impervious Area notes on sheet C102.
13. Please revise notes #11, #14 and #15 on sheet C101 to reflect the following verbiage:
 - #11: All traffic control signs and markings off the right-of-way are to be maintained by the property owner **in accordance with MUTCD standards**.
 - #14: A utility cut permit is required for each open cut of a City street. Note this on the plan and contact 341-5888 for more details. In certain cases an entire resurfacing of the area being open cut may be required.
 - #15: Any broken or missing sidewalk panels, driveway panels and curbing will be replaced.

GENERAL NOTES TO ADD TO THE PLAN:

- A. A landscaping plan indicating the location of required street trees shall be submitted to the City of Wilmington Traffic Engineering Division and Parks and Recreation Department for review and approval prior to the recording of the final plat. [\[SD 15-14 CofW Tech Stds\]](#)

MISCELLANEOUS:

- ❖ Permitting of business identification signage is a separate process. NCDOT/ City of Wilmington will not allow obstructions within the right of way.
- ❖ Contact 341-7888 to discuss street lighting options.
- ❖ Transportation Planning Staff will review the landscape plan once submitted for compliance with sight distance requirements.
- ❖ Include appropriate City of Wilmington approval stamps prior to submitting final plans.
- ❖ Transportation Planning Staff reserves the right to review and comment on any additional plans submitted for this development.

Please let me know if you have any questions or if I can be of further assistance as this development moves through the review process.