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DATE: 02.18.2015
TO: ProTrak
FROM: Bill McDow
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■ **BRAGG ROAD DEVELOPMENT COMPANY [TRC Plan Review]**

↻ Initial Review Note ↻

All comments are based on adopted policy documents, specific City of Wilmington Land Development Code (CofW LDC) and City of Wilmington Technical Standards and Specifications Manual (CofWTSSM) requirements in effect at the time of this review. These have been noted after the comment as appropriate for your ease in further research.

The City Engineer may administratively approve dimensional variance requests per LDC Sec. 18-348. If the Applicant feels further interpretation is in order, please contact Transportation Planning directly to discuss in additional detail. Please apply for all administrative variances in writing, an email is acceptable or this may be included on the plan set. Please see Sec. 18-348 of the Land Development Code for the information required to process a variance request.



BASE INFORMATION:

- Revise all sheets to accurately illustrate the existing conditions on College Rd and surrounding this parcel. Show all adjacent traffic signs, lane configurations, traffic control devices and pavement markings on the site plan. Please refer to the Technical Standards and Specifications Manual (link is below) Ch. VII C.1.a.2. for specific information and distance requirements. Contact Traffic Engineering at 341-7888 if you need assistance locating these elements.

TRAFFIC IMPACT:

- ❖ A Traffic Impact Analysis (TIA) is required for this development. Amy Kimes, PE, is the contact for the TIA process. Please contact her to discuss the TIA review process and scoping discussion at (910) 473-5130, or email her amy.kimes@wilmingtonnc.gov.

NCDOT:

- It is the Applicant’s responsibility to coordinate directly with NCDOT to determine if any driveway permits/ revisions/ review processes are required. Contact Allen Hancock, PE at 910. 251.2655. Please note additional comments may be forthcoming once coordination occurs.
- The proposed College Rd Access is dependent on NCDOT approval. If the access is not approved, the current site plan will have to be revised.

TECHNICAL STANDARDS – DRIVEWAY ACCESS (Major Thoroughfares):

1. Traffic Engineering’s stance on sites that interconnect along a major thoroughfare is that the combined frontage must meet all driveway spacing requirements per the City’s technical standards.

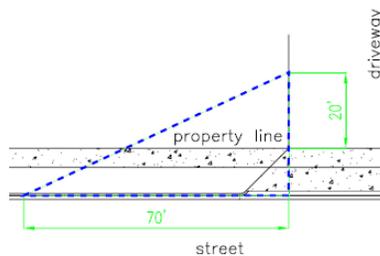
- The proposed College Rd driveway does not have the 75' offset from property lines to the curb line required for lots along major thoroughfares. The spacing for this driveway is too close to the Bank of America Driveway.

TECHNICAL STANDARDS – DRIVEWAY ACCESS (Non-Major Thoroughfares):

- The proposed driveway access to the furniture store appears to be blocked by the proposed exit lane to College Rd.
- The driveway curb return must be at least 6.5' from intersecting property lines. [[Chapter VII, C, Table 4 CofWTSSM](#)]

TECHNICAL STANDARDS – ACCESS (driveway, sidewalk, sight distance):

- Show proposed driveway centerline elevations at both the front and the back of each concrete driveway apron, at the curb line, property line & points at 26' and 52' behind the property line. [[Chapter VII, C\(1\)\(a\)\(2\)12 CofWTSSM](#)]
- Dimension driveway widths and tapers. [[Sec.18-529 CofW LDC](#)]
- The minimum two-lane driveway width is 23'. [[7-9 CofW Tech Stds](#)]
- The maximum two-lane driveway width is 30'. [[7-9 CofW Tech Stds](#)]
- The maximum three-lane driveway width is 36'. [[7-9 CofW Tech Stds](#)]
- A pavement marking plan is required for all driveways greater than 36' in width.
- Provide a sidewalk connection between the site and the public sidewalk on College Rd.
- Distinguish between proposed and existing sidewalk(s) and provide dimensions
- Provide internal sidewalks within the parking lot. The one way drive aisles for 60 degree parking may be reduced to allow for a walkway between the angled parking.
- The two way drive aisle near the truck loading dock appears to be less than 24' wide.
- Show any traffic signal facilities and equipment in front of the development. If fiber optic cable or overhead cable is present, show it on the site plans. Contact Traffic Engineering at 341-7888 for help in identifying these.
- Show and apply the City's 20'x70' sight distance triangle at each driveway on the site plan and landscaping plan. [[Sec.18-529\(c\)\(3\)CofW LDC](#)] Add a note indicating that all proposed vegetation within sight triangles shall not interfere with clear visual sight lines from 30"-10'. [[Sec.18-566 CofW LDC](#)]



TECHNICAL STANDARDS – PARKING:

- Dimension parking stalls, radii and parking aisles for the angled parking area. [[Sec. 18-529\(b\)\(2\) CofW LDC](#)]
- The minimum standard vehicle parking space size for 60 degree angled parking is 8.5' in width by 20' in length.
- Fourteen feet (14') is the minimum drive aisle width behind 60 degree angled parking. The additional drive aisle space may be used for pedestrian walkways between the parking spaces.
- The front of the building appears to have a Drop off/pick up area. If the applicant is using this area for customer drop off or package pickup, pavement markings will be required to separate this area from the standard two-way drive aisle. [[Sec.18-553 CofW LDC](#)]

21. The proposed handicap accessible parking spaces do not provide the required 96" access aisle required for van accessible spaces. If the applicant desires to modify the space from 12' to 9' the additional 3' may be used for the access aisle.
22. Protection from vehicles is required around all required landscaped areas within vehicular areas. The striped island is not adequate protection.
23. Show the location of proposed street lights on the site plan.
24. Portions of the proposed development do not appear to be in compliance with the 500'/800' rule pertaining to the maximum length of a private drive/parking lot. [[Chapter VII , C \(4\), pg 7-15 to 7-16 CofWTSSM](#)]
25. Provide bicycle parking in accordance with Section 18-528 of the City of Wilmington Land development Code. Bike parking is based on provided spaces, not the minimum required spaces. Add the number required and proposed to the site development data and graphically indicate on the plans the location of bike parking. [[Sec.18-528 CofW LDC](#)]
26. The proposed landscaping plan does not save any local trees. Spade and relocate or modify site plan to save regulated trees on the site. [[Sec.18-282 CofW LDC](#)]
27. The landscaping plan appears to numerous trees within the 20'X70' sight distance triangle that will interfere with clear visual sight distance from 30" to 10' for the Bragg Dr entrances.
28. Please apply the sight distance triangle for the landscaping islands at the end of the two way drive aisle at the Eastern end of the parking lot. Clear visual sight lines must be provided to let drivers see approaching traffic from the proposed College Rd entrance.
29. Please verify the location of the Bragg Dr sidewalk on the landscaping plan.

TECHNICAL STANDARDS – Barrier Free Design:

30. Please show location of all handicap ramp(s) and signs and provide details on the plan.
31. Note/label the plan with spot elevations that clearly indicate the accessible route from the HC space(s) to the building. [[Sec. 18-529\(b\)\(2\) CofW LDC](#)]
32. Please show location of the stairs and provide details on the site plan.
33. Show the location of the proposed bollards on the site plan.

REVISIONS TO NOTES ON THE PLAN:

34. Please revise notes #11 and #15 on sheet C001 to reflect the following verbiage:
 - #11: All traffic control signs and markings off the right-of-way are to be maintained by the property owner **in accordance with MUTCD standards.**
 - #15: Any broken or missing sidewalk panels, driveway panels and curbing will be replaced.

MISCELLANEOUS:

- ❖ Permitting of business identification signage is a separate process. NCDOT/ City of Wilmington will not allow obstructions within the right of way.
- ❖ Contact 341-7888 to discuss street lighting options.
- ❖ Transportation Planning Staff reserves the right to review and comment on any additional plans submitted for this development.

Please let me know if you have any questions or if I can be of further assistance as this development moves through the review process.