



**Planning, Development
and Transportation**

Transportation Planning
305 Chestnut Street
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DATE: 02.12.2019
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■ **WOODLANDS AT ECHO FARMS TRACT 3A [TRC Plan Review #2]**

NEW ROADS:

1. Please show the full centerline pavement markings for the proposed street.
2. A pedestrian crossing is shown connecting clubhouse and mail kiosk. Sight distance easements and the landscaping plan will be required to provide AASHTO safe stopping distance to the crosswalk.
3. A raised center median/pedestrian refuge with In-street Pedestrian Crossing Signs (R1-6) and Pedestrian Crossing (W11-2 and W16-7P) are recommended for this crosswalk as per MUTCD.
4. An enhanced advanced warning of the crossing for northbound traffic, in addition to the standard MUTCD signage will be required.
5. A handicap signage for a handicap space to the north of the clubhouse is shown within Midvale Dr ROW. Please relocate this sign outside the ROW.
6. In previous submittal, a mid-block crossing was proposed across Chastain Dr. If that crosswalk is planned to be installed along the future projects, all the relevant comments from previous review would be applicable.

ROUNDBABOUT – Belfairs Dr Intersection:

7. Please show the approach sight distance, intersection sight distance and sight distance to crosswalk for this roundabout.
8. Dimension the stopping sight distances to crosswalk and show the design speed and sight distance relationship calculation.
9. A refuge island is shown at the Midvale Dr end of roundabout. At cut-through pedestrian refuge islands, detectable warning surfaces shall begin at the curb line and extend into the cut-through area. It must not protrude into the gutter section.
10. If cut-through pedestrian refuge islands are less than 6 ft in length detectable warning domes are not required because detectable warning surfaces must extend 2 ft minimum on each side of the island and be separated by 2 ft minimum. Please verify these distances and provide the dimensions.

ROUNDBABOUT – Midvale Dr and Chastain Dr:

11. Refuge islands are shown at this roundabout. At cut-through pedestrian refuge islands, detectable warning surfaces shall begin at the curb line and extend into the cut-through area. It must not protrude into the gutter section.
12. If cut-through pedestrian refuge islands are less than 6 ft in length detectable warning domes are not required because detectable warning surfaces must extend 2 ft minimum on each side of the island and be separated by 2 ft minimum. Please verify these distances and provide the dimensions.
13. Dimension the stopping sight distances to crosswalk and show the design speed and sight distance relationship calculation.
14. If the central island is designed to be fully traversable, atleast one of the traffic sign shown within an island is in conflict with the left-turn movement of the designed vehicle (based on the *Autoturn exhibit*). Relocate the sign, if required.

15. Autoturn exhibit:

- a) Please show the type of the fire truck and garbage truck used for the turning template.
- b) Garbage truck is shown as acquiring more space while maneuvering over central mountable island, as compared to fire truck. Please verify and explain.

TECHNICAL STANDARDS – ACCESS (driveway, sidewalk, and sight distance):

16. Show and apply the City’s 20’x70’ sight distance triangle at the driveway off Chastian Dr roundabout.

TECHNICAL STANDARDS – Barrier Free Design:

- 17. Relocate the accessible parking sign in front of building #9, from accessible aisle to accessible space.
- 18. Handicap signs are shown at the incorrect place for the spaces in front of building #15. Please relocate in front of the handicap spaces.

STREET LIGHTING [City of Wilmington Street Lighting Policy]:

1. A layout for the Standard street lighting provided with the previous subdivision review is still applicable. Minimum of six street lights are required for this sub-division. Revise on the site plan as required. Although, developers may choose to provide any extra lights or ornamental. Any installations above the criteria of Standard street lighting, will be considered as non-standard and has to conform to the City’s non-standard street lighting procedure as per the policy.

WOODLANDS AT ECHO FARMS TRACT 3A Standard Street Lighting Plan

1. Developers shall bear any installation costs associated with streetlights. If applicable, in areas served from underground facilities, the developers shall pay the one-time underground contribution charge, if applicable.

2. Streetlights will be reviewed through the subdivision process and must meet criteria established in City's Street Lighting Policy.

3. If any of the existing street lights coincide with the proposed, required number of street lights must be amended to comply with the street lighting policy.

● Suggested approximate locations of minimum required six standard street lights. Locations may vary as per the field survey by DEP.

08/07/2018

PROPOSED BLDG HEIGHT	# OF UNITS/BLDG	# OF BDRMS/UNIT	# OF RECD PARKING SPACES/BLDG
32' + 10'	8	2	20
30' + 4'	20	1	30
32' + 10'	5	2	15
32' + 10'	10	2	25
43' + 4'	9	3	SELF-PARKED WITH GARAGES & DRIVEWAYS
30' + 4'	16	1	24
32' + 10'	8	2	20
43' + 4'	8	3	SELF-PARKED WITH GARAGES & DRIVEWAYS
TOTAL PARKING SPACES REQUIRED (MINIMUM) =			323

Please let me know if you have any questions or if I can be of further assistance as this development moves through the review process.