



**Planning, Development
and Transportation**
Transportation Planning
305 Chestnut Street
PO Box 1810
Wilmington, NC 28402-1810

910 341-3258
910 341-7801 fax
www.wilmingtonnc.gov
Dial 711 TTY/Voice

DATE: 01.20.2016
TO: ProTrak
FROM: Bill McDow
Transportation Planning

■ **THE FLATS AT MAIN [TRC Plan Review]**

↻ Initial Review Note ↻

All comments are based on adopted policy documents, specific City of Wilmington Land Development Code (CofW LDC) and City of Wilmington Technical Standards and Specifications Manual (CofWTSSM) requirements in effect at the time of this review. These have been noted after the comment as appropriate for your ease in further research.

The City Engineer may administratively approve dimensional variance requests per LDC Sec. 18-348. If the Applicant feels further interpretation is in order, please contact Transportation Planning directly to discuss in additional detail. Please apply for all administrative variances in writing, an email is acceptable or this may be included on the plan set. Please see Sec. 18-348 of the Land Development Code for the information required to process a variance request.

BASE INFORMATION:

- Site Notes #24 and #25 are substantially the same, therefore, #24 can be removed.
- Thank you for referencing the Administrative Variance for 14 parking spaces beyond the 500' rule. This Administrative Variance request will be processed along with this plan review.
- This site entrance to Mayfaire Flats requires changes to this site. Show parking spaces and curb to be removed from the Mayfaire Flats 1 on the demo page.

TRAFFIC IMPACT:

- ❖ Please provide estimated Trip Generation data for this development per the Institute of Transportation Engineers (ite) Trip Generation 9th Edition, 2012, Microtrans trip Generation Software. The estimated Trip Generation should follow an example of proposed land use, [**LIST PROPOSED USE + INTENSITY**] (ite land use code xxx) is xx total driveway volume in the AM peak hours, xx total driveway volume in the PM peak hours and xx average weekday 2-way driveway volume.

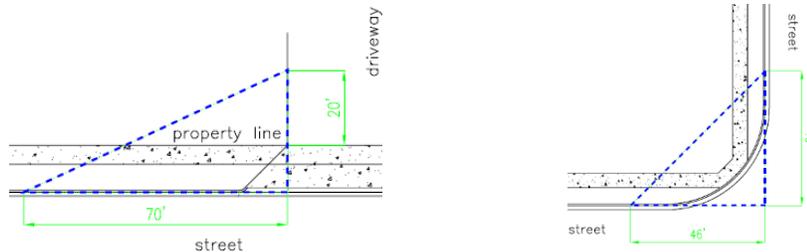
TECHNICAL STANDARDS – NEW ROADS:

1. Please provide a street name for the proposed Asphalt Street which provided access to Tract F and will provide future connection to Old MacCumber Station Road.
2. Show a cross-section detail showing the proposed street ROW width. A variance for a non-standard street design, (ROW width and lack of street plaza & sidewalk) may be required.
3. The proposed street connection to Sir Tyler Dr. requires a street type radius and apron.
4. Increase the proposed on-street parking width from 7' to 8'. [[7-16, Section D CofW Tech Stds](#)]
5. Any multi-family development of more than 50 units must be served by a public street.
6. The minimum street corner radii is 35'. [[7-5 CofW Tech Stds](#)]

TECHNICAL STANDARDS – ACCESS (driveway, sidewalk, and sight distance):

7. Dimension driveway widths and tapers. [[Sec.18-529 CofW LDC](#)]

8. Please change the driveway connections from the site to the new street to regular driveway aprons.
9. The proposed sidewalk connection between the site and the public sidewalk on Sir Tyler Drive is shown as 4'. Increase the sidewalk width to at least 5'.
10. Show all adjacent traffic signs and pavement markings on the plan. [Sec. 18-529(b)(2) CofW LDC]
11. Ensure proposed street trees for the Sir Tyler Dr. Street yard do not block clear sight distance at the new street entrance. A 46'x46' SDT will be applied at this location.
12. Show and apply the City's 20'x70' sight distance triangle at each driveway and the City's 46'x46' sight distance triangle at each street corner intersection on the site plan and landscaping plan. [Sec.18-529(c)(3)CofW LDC] Add a note indicating that all proposed vegetation within sight triangles shall not interfere with clear visual sight lines from 30"-10'. [Sec.18-566 CofW LDC]



TECHNICAL STANDARDS – PARKING:

13. Dimension parking radii for the parking islands adjacent to the Mayfaire Flats 1 entrance.
14. Please distinguish between areas of 18" curb and gutter and 24" curb and gutter on the site.
15. The minimum radius is 25' for any portion of a parking lot adjacent to a travel way (i.e. islands at the end of a parking bay) for parking that is open to the public. If the travel way will not be used for emergency service vehicles or truck traffic, you may request a tighter radius, the minimum is 15'. This request must be made in writing (an email message is acceptable). [Chapter VII, Detail SD 15-13 CofWTSSM]
16. As stated in the Site Notes, portions of the proposed development do not appear to be in compliance with the 500'/800' rule pertaining to the maximum length of a private drive/parking lot. [Chapter VII , C (4), pg. 7-15 to 7-16 CofWTSSM]

TECHNICAL STANDARDS – Barrier Free Design:

17. Revise the handicapped parking sign detail to show the R7-8D Van Accessible sub-plate below the standard R7-8 sign.
18. Please change the Van Accessible parking space and parking aisle from 11' and 5' to the 8' space and 8' parking aisle listed in ADA guidelines. [www.ada.gov/restripe]
19. Please update the number of required and proposed handicapped spaces in the site data table. [Sec. 18-529(b)(2) CofW LDC]

GENERAL NOTES TO ADD TO THE PLAN:

- A. A sign shall be placed at the northern boundary of the property indicating that a future connection will be opened when the property to the north develops. Please note this on the site plan.

MISCELLANEOUS:

- ❖ Permitting of business identification signage is a separate process. City of Wilmington will not allow obstructions within the right of way.

Please let me know if you have any questions or if I can be of further assistance as this development moves through the review process.