

SITE CONSTRUCTION PLANS

SQUARE TWO

1602 QUEEN STREET
WILMINGTON, NC 28403

INFILL PROJECT

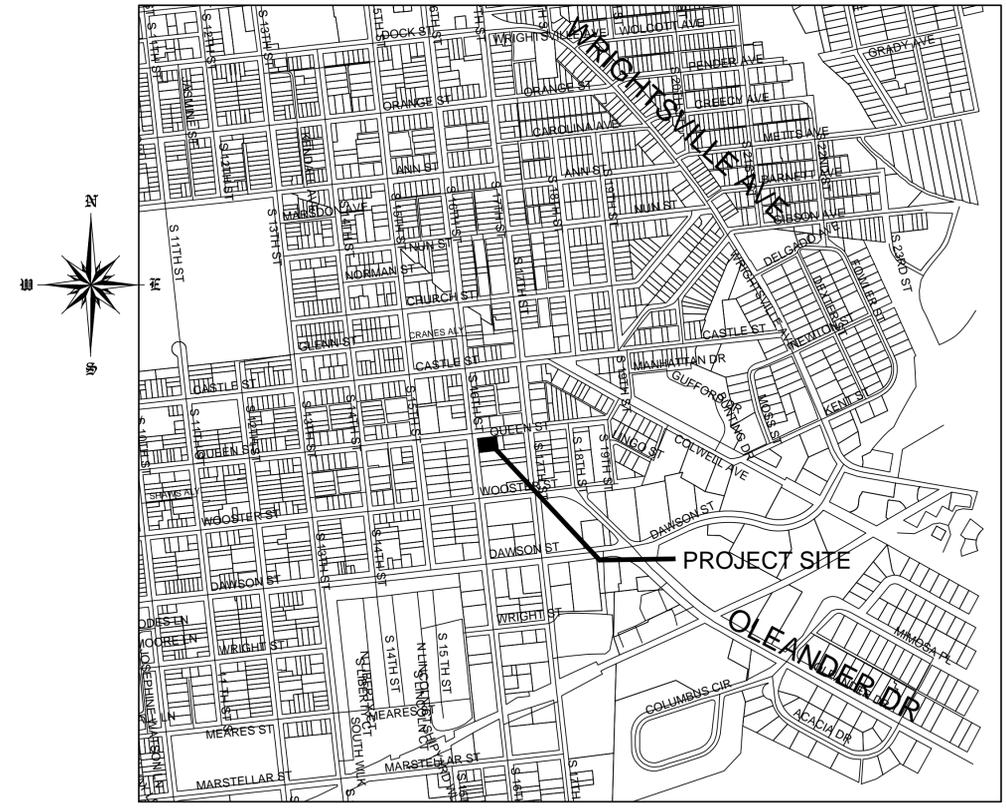
NOTES

1. PRIOR TO ANY CLEARING, GRADING OR CONSTRUCTION ACTIVITY, TREE PROTECTION FENCING (IF REQUIRED) WILL BE INSTALLED AROUND PROTECTED TREES OR GROVES OF TREES AND NO CONSTRUCTION WORKERS, TOOLS, MATERIALS, OR VEHICLES ARE PERMITTED WITHIN THE TREE PROTECTION.
2. ANY TREES AND/OR AREAS DESIGNATED TO BE PROTECTED MUST BE PROPERLY BARRICADED WITH FENCING AND PROTECTED THROUGHOUT CONSTRUCTION TO INSURE THAT NO CLEARING, GRADING OR STAGING OF MATERIALS OCCUR IN THOSE AREAS.
3. NO EQUIPMENT IS ALLOWED ON SITE UNTIL ALL TREE PROTECTION FENCING AND SILT FENCING IS INSTALLED AND APPROVED. PROTECTIVE FENCING IS TO BE MAINTAINED THROUGHOUT THE DURATION OF THE PROJECT, AND CONTRACTORS SHALL RECEIVE ADEQUATE INSTRUCTION ON TREE PROTECTION METHODS.
4. ALL PAVEMENT MARKINGS IN PUBLIC RIGHTS OF WAY AND FOR DRIVEWAY ARE TO BE THERMO PLASTIC AND MEET CITY AND/OR NCDOT STANDARDS.
5. ONCE STREETS ARE OPEN TO TRAFFIC, CONTACT TRAFFIC ENGINEERING REGARDING THE INSTALLATION OF TRAFFIC AND STREET NAME SIGNS. PROPOSED STREET NAMES MUST BE APPROVED PRIOR TO INSTALLATION OF STREET SIGN NAMES.
6. TRAFFIC CONTROL DEVICES (INCLUDING SIGNS AND PAVEMENT MARKINGS) IN AREAS OPEN TO PUBLIC TRAFFIC ARE TO MEET MUTCD STANDARDS AND BE MAINTAINED BY THE OWNER.
7. CONTACT TRAFFIC ENGINEERING AT 910-341-7888 TO ENSURE THAT ALL TRAFFIC SIGNAL FACILITIES AND EQUIPMENT ARE SHOWN ON THE PLAN.
8. TRAFFIC ENGINEERING TO BE NOTIFIED AT 341-7888 FORTY-EIGHT HOURS PRIOR TO ANY EXCAVATION IN THE RIGHT OF WAY.
9. TRAFFIC ENGINEERING MUST APPROVE OF PAVEMENT MARKING PRIOR TO ACTUAL STRIPING.
10. ALL PARKING STALL MARKINGS AND LANE ARROWS WITHIN PARKING AREAS SHALL BE WHITE. ALL PAVEMENT MARKINGS IN PUBLIC RIGHTS OF WAY FOR DRIVEWAY ARE TO BE THERMO PLASTIC AND MEET CITY AND/OR NCDOT STANDARDS.
11. ALL TRAFFIC CONTROL SIGNS AND MARKINGS OFF THE RIGHT OF WAY ARE TO BE MAINTAINED BY THE PROPERTY OWNER.
12. STOP SIGNS AND STREET SIGNS TO REMAIN IN PLACE DURING CONSTRUCTION.
13. TACTILE WARNING MATS WILL BE INSTALLED ON ALL WHEEL CHAIR RAMPS.
14. A UTILITY CUT PERMIT IS REQUIRED FOR EACH OPEN CUT OF A CITY STREET.
15. ANY BROKEN OR MISSING SIDEWALK PANELS, DRIVEWAY PANELS AND CURBING WILL BE REPLACED.
16. CONTACT TRAFFIC ENGINEERING AT 341-7888 TO DISCUSS STREET LIGHTING OPTIONS.
17. WATER AND SEWER SERVICE SHALL MEET CAPE FEAR PUBLIC UTILITY AUTHORITY DETAILS AND SPECIFICATIONS.
18. PROJECT SHALL COMPLY WITH CFPWA CROSS CONNECTION CONTROL REQUIREMENTS. WATER METERS CANNOT BE RELEASED UNTIL ALL REQUIREMENTS ARE MET AND THE STATE HAS GIVEN THEIR FINAL APPROVAL.
19. IF THE CONTRACTOR DESIRES CFPWA WATER FOR CONSTRUCTION HE SHALL APPLY IN ADVANCE FOR THIS SERVICE AND MUST SUPPLY A RPZ BACKFLOW PREVENTION DEVICE ON THE DEVELOPER'S SIDE OF THE WATER METER BOX.
20. ANY IRRIGATION SYSTEM SUPPLIED BY CFPWA SHALL COMPLY WITH THE CFPWA CROSS CONNECTION REGULATIONS.
21. ANY IRRIGATION SYSTEM SHALL BE EQUIPPED WITH A RAIN AND FREEZE SENSOR.
22. ANY BACKFLOW PREVENTION DEVICES REQUIRED BY THE CFPWA WILL NEED TO BE ON THE LIST OF APPROVED DEVICES BY USCFCOCHR.
23. CONTRACTOR TO FILED VERIFY EXISTING WATER AND SEWER LOCATIONS, SIZES AND MATERIALS PRIOR TO CONSTRUCTION. ENGINEER TO BE NOTIFIED OF CONFLICTS.
24. AN ALL WEATHER DRIVING SURFACE FOR EMERGENCY VEHICLES MUST BE MAINTAINED DURING CONSTRUCTION.
25. UNDERGROUND FIRE LINE(S) MUST BE PERMITTED AND INSPECTED BY THE WILMINGTON FIRE DEPARTMENT FROM THE PUBLIC ROW TO THE BUILDING. CONTACT THE WILMINGTON FIRE DEPARTMENT/DIVISION OF FIRE AND LIFE SAFETY AT 910-341-0696.
26. NO OBSTRUCTIONS ARE PERMITTED IN THE SPACE BETWEEN THIRTY (30) INCHES AND TEN (TEN) FEET ABOVE THE GROUND WITHIN THE TRIANGLE SIGHT DISTANCE.
27. CONTRACTOR IS TO NOTIFY (AT A MINIMUM) THE FOLLOWING COMPANIES FOR LOCATION SERVICES:
ULOCO1-800-632-4949, 48 HOURS IN ADVANCE
BELL SOUTH.....1-800-392-8712 (STEVE DAYVAULT)
CAROLINA ONE CALL CENTER 1-800-632-4949
28. THERE SHALL BE NO ENCUMBRANCES WITHIN THE NCDOT RIGHT OF WAY.
29. A LANDSCAPING PLAN INDICATING THE LOCATION OF REQUIRED STREET TREES SHALL BE SUBMITTED TO THE CITY OF WILMINGTON TRAFFIC ENGINEERING DIVISION AND PARKS AND RECREATION DEPARTMENT FOR REVIEW AND APPROVAL PRIOR TO THE RECORDING OF THE FINAL PLAT (SD 15-14 CoW TECH STDS)
30. FIRE HYDRANTS CANNOT BE FURTHER THAN 8' FROM THE CURB.
31. PARKING AND LANDSCAPING CANNOT BLOCK FIRE HYDRANTS OF FDC'S
32. FDC'S CAN BE NO FURTHER THAN 40' FROM FIRE DEPARTMENT VEHICLE PLACEMENT
33. FDC'S CAN BE NO FURTHER THAN 150' FROM A FIRE HYDRANT
34. BICYCLE PARKING IS PROVIDED ON THE SOUTHWEST CORNER OF PROPOSED BUILDING

Approved Construction Plan	
Name	Date
Planning	
Traffic	
Fire	

DRAINAGE PLAN APPROVED CITY OF WILMINGTON	
STORMWATER DISCHARGE PERMIT NOT REQUIRED	
SIGNATURE	DATE

For each open utility cut of City streets, a \$325 permit shall be required from the City prior to occupancy and/or project acceptance.



SCALE 1" = 600'
VICINITY MAP
AUGUST, 2016

LIST OF DRAWINGS

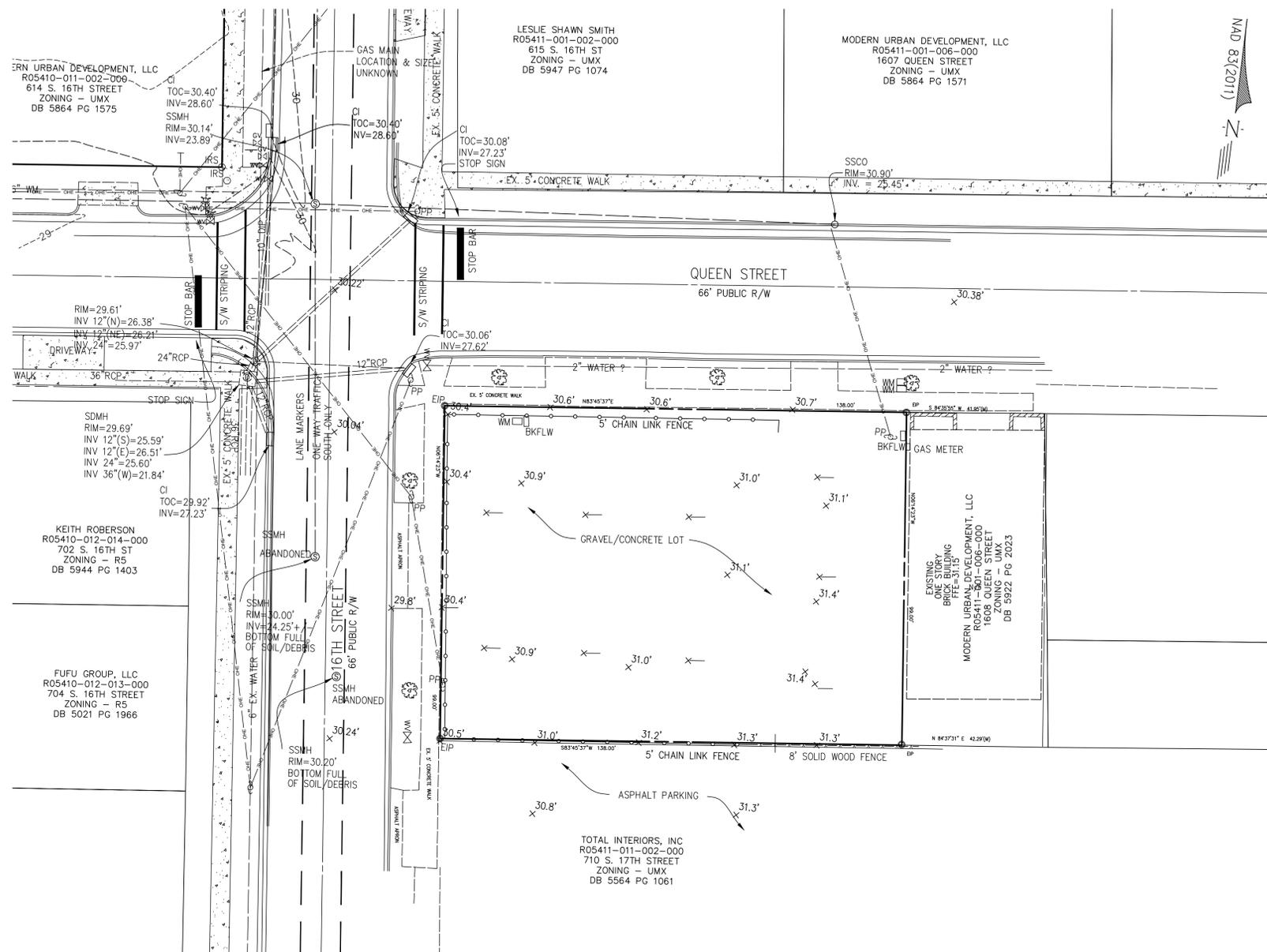
XX	COVER SHEET
C1	EXISTING SITE
C2	SITE PLAN
C3	GRADING, DRAINAGE & EROSION CONTROL
C4	UTILITIES
C5	LANDSCAPE PLAN
C6	DETAILS
A1	BUILDING ELEVATION

OWNER/DEVELOPER
L.S. SMITH, INC.
MR. LESLIE SMITH
1607 QUEEN STREET
WILMINGTON, NC 28401
1-336-404-0009

CIVIL ENGINEER
JBS CONSULTING, PA
BRAD SEDGWICK, PE
7332 COTESWORTH DRIVE
WILMINGTON, NC 28405
1-910-619-9990

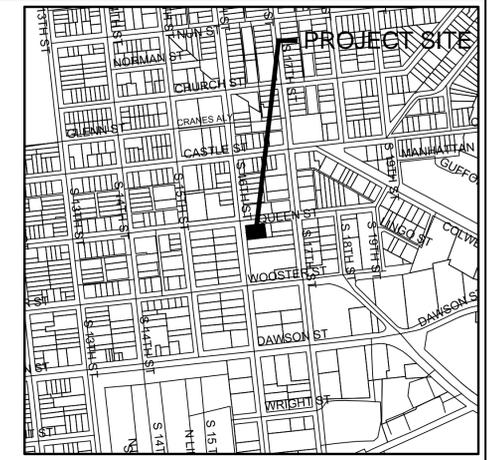
SURVEYOR
ROBERT SESSOMS & ASSOCIATES, PLLC
MR. ROBERT SESSOMS, PLS
4033 CHANDLER DRIVE
WILMINGTON, NC 28405
1-910-352-8846





SITE DATA

TOTAL PROJECT AREA	13,504 SF
EXISTING_BUA	
ENTIRE LOT IS GRAVEL/ CONCRETE PAD	13,504 SF



VICINITY MAP
NTS

DESIGN BY:	BDS
DRAWN BY:	BDS
CHECKED BY:	BDS
DATE	AUGUST, 2016

EXISTING SITE
SQUARE TWO
WILMINGTON, NC

L S SMITH, INC.
1607 Queen Street
Wilmington, NC 28401-5526
910-409-8782



JBS CONSULTING, PA
7332 Colesworth Drive
Wilmington, NC 28405
(910) 619-9990
License Number C-2525
PLANNING - ENGINEERING - PROJECT MANAGEMENT

SHEET
C1
JOB NO. 104-001

EXISTING SITE
SCALE 1" = 20'

LEGEND

EX. GRAVEL	
ASPHALT PAVEMENT	
CONC. PAVEMENT	
EXISTING SPOT ELEVATIONS	X 41.5'
WAVE STYLE BIKE RACK 8 BIKE SPOTS EACH	
TREE PROTECTION FENCING SEE DETAIL	
TREE FOR REMOVAL	



Approved Construction Plan

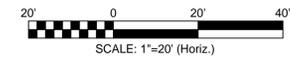
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DRAINAGE PLAN
APPROVED
CITY OF WILMINGTON

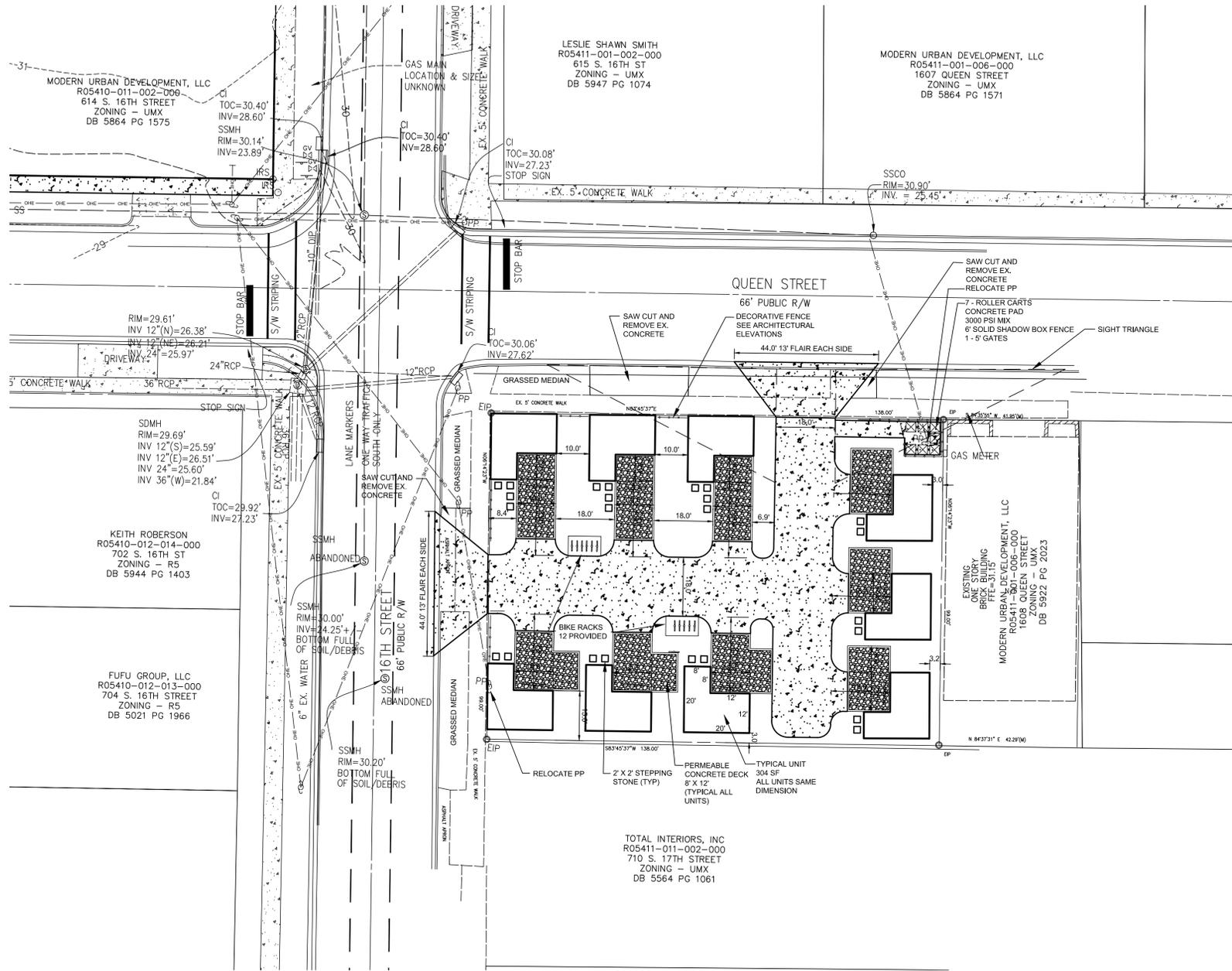
STORMWATER DISCHARGE PERMIT NOT REQUIRED

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REVISION	
NUMBER	DATE



SITE DATA

ADDRESS 1602 QUEEN STREET
 WILMINGTON, NC 28403-2343
 PARCEL ID NUMBERS R05411-011-006-000
 DEED BOOK & PAGE D.B. 5922 PG. 2023
 MAP BOOK & PAGE MB 2 PG. 56
 ZONING UMX
 CITY OF WILMINGTON
 TOTAL LOT AREA 0.31 ACRES - 13,504 SF
 CAMA LAND USE CLASSIFICATION .. URBAN
 OVERLAY DISTRICT - PROPERTY IS NOT IN A SPECIAL HIGHWAY OVERLAY DISTRICT
 PROPOSED USE.....MULTI FAMILY

	REQUIRED	PROPOSED
MIN. LOT AREA	N/A	0.31 ACRES
MIN. LOT WIDTH	N/A	137'
MAX. LOT COVERAGE	N/A	20.3%
FRONT BUILDING SETBACK	N/A	0'
REAR BUILDING SETBACK	N/A	0'
SIDE BUILDING SETBACK	N/A	0'
RIGHT SIDE	N/A	0'
LEFT SIDE	N/A	0'
MAX. BUILDING HEIGHT	45'	17'

LOT COVERAGE..... 20.3%

TOTAL ONSITE NEWLY CONSTRUCTED IMPERVIOUS SURFACE

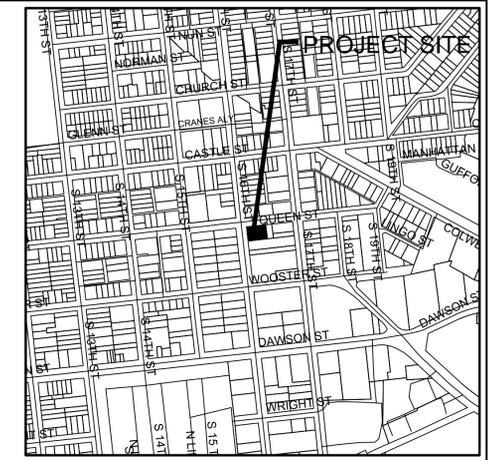
BUILDINGS	2,736 SF
IMPERVIOUS PAVEMENT	4,146 SF
PERVIOUS PAVEMENT	2,683 SF
IMPERVIOUS SIDEWALKS	0 SF
PERVIOUS SIDEWALKS	0 SF
OTHER	0 SF
TOTAL ONSITE NEWLY CONSTRUCTED IMPERVIOUS SURFACE	8,236 SF

PARKING REQUIREMENTS
 NO PARKING REQUIREMENTS PER SECTION 18-204 OF WILMINGTON CITY CODE

SOLID WASTE - FACILITY WILL BE SERVED WITH ROLLER CARTS AS SHOWN

UTILITY INFORMATION

WATER EXISTING USAGE	0 GPD
(9 - 1 BORM UNITS BUT MINIMUM 240 GPD USAGE)	
WATER USAGE PROPOSED	2,160 GPD
SEWER USAGE - EXISTING	0 GPD
SEWER USAGE - PROPOSED	2,160 GPD



VICINITY MAP
NTS

SITE PLAN
SCALE 1" = 20'

LEGEND

ASPAHLT PAVEMENT	
CONC. PAVEMENT	
PROPOSED SPOT ELEVATION	× 41.5'
WAVE STYLE BIKE RACK & BIKE SPOTS EACH	
TREE PROTECTION FENCING SEE DETAIL	



Approved Construction Plan

Name	Date
Planning	
Traffic	
Fire	

DRAINAGE PLAN
APPROVED
CITY OF WILMINGTON

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 CHECKED BY: BDS
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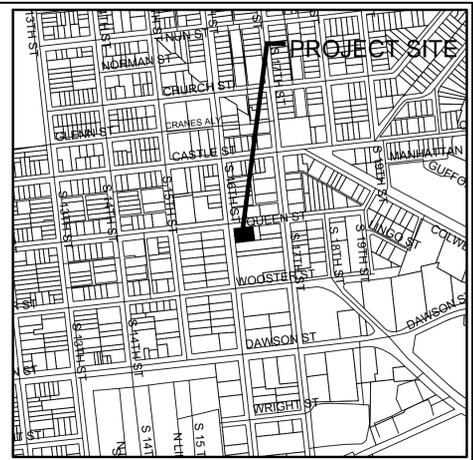
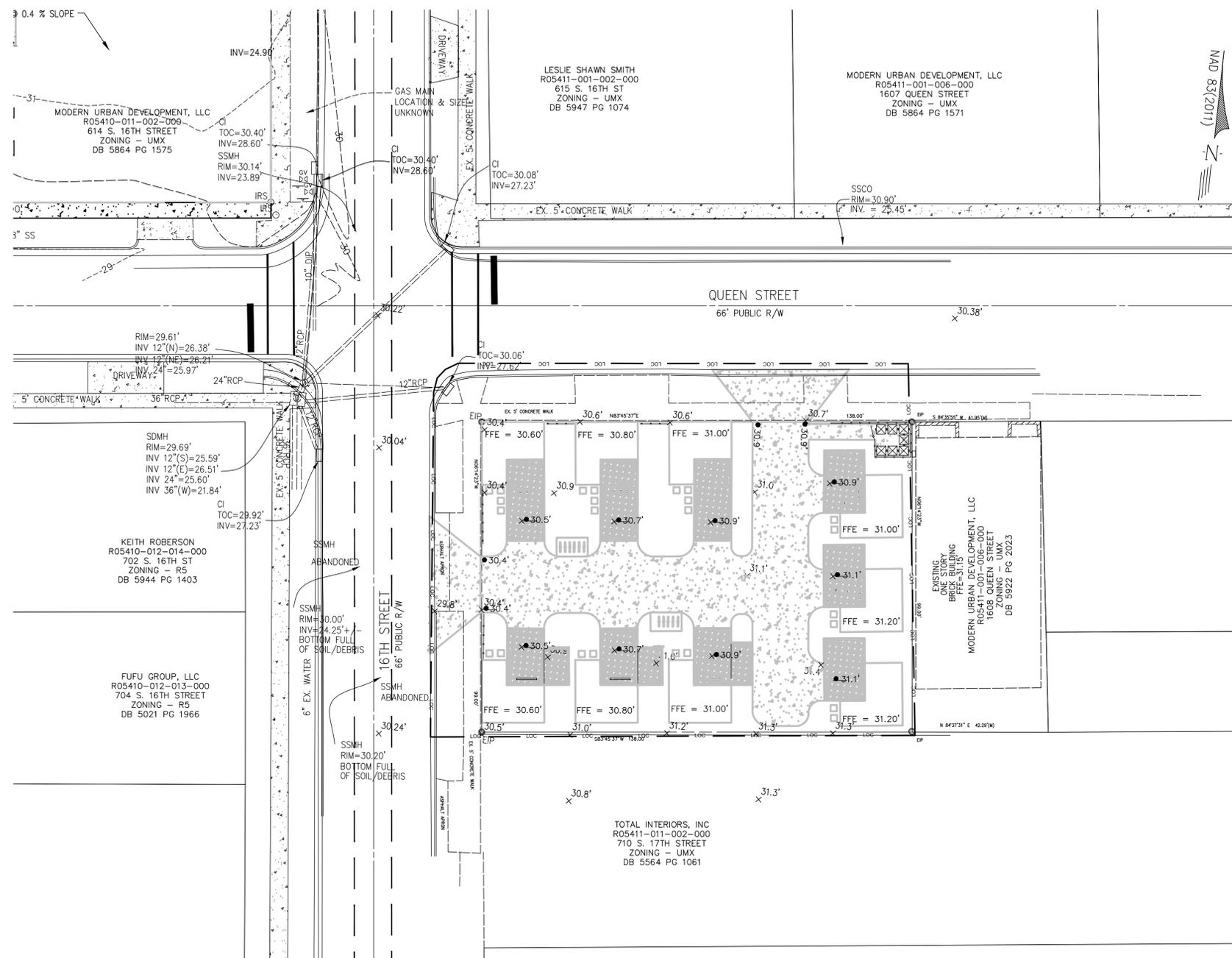
SITE PLAN
SQUARE TWO
WILMINGTON, NC

L S SMITH, INC.
 1607 Queen Street
 Wilmington, NC 28401-5526
 910-409-8782



JBS CONSULTING, PA
 7332 Catesworth Drive
 Wilmington, NC 28405
 (910) 619-9990
 License Number C-2525
 PLANNING - ENGINEERING - PROJECT MANAGEMENT

SHEET
C2
 of 6
 JOB NO. 104-002



VICINITY MAP
NTS

GRADING NOTES

- INITIATE EROSION CONTROL SEQUENCE BEFORE BEGINNING CLEARING, GRUBBING AND GRADING OPERATIONS.
- CLEAR AREAS TO BE GRADED OF ALL VEGETATION. PROTECT VEGETATION BEYOND GRADING LIMITS.
- STRIP TOPSOIL TO FULL DEPTH IN AREAS TO BE GRADED AND STOCKPILE.
- COMPACT ALL FILL AREAS TO 95% OF MAXIMUM DENSITY.
- ALL BANKS AND SWALE SIDE SLOPES SHALL BE GRADED WITH NO GREATER THAN 3:1 SLOPES.
- ALL AREAS ARE TO BE GRADED SO THAT NO AREAS OF STANDING WATER OCCUR.
- PROPOSED SPOT ELEVATIONS ARE SHOWN AT FINISHED GRADE.
- OPERATOR SHALL FIELD VERIFY EXISTING TOPOGRAPHY IN RELATION TO THE PROPOSED GRADES TO ENSURE DRAINAGE IN THE DIRECTIONS INDICATED ON THE PLAN.

CONSTRUCTION SEQUENCE

- OBTAIN ALL NECESSARY PERMIT APPROVALS PRIOR TO ANY LAND DISTURBING ACTIVITY.
- HOLD A PRE-CONSTRUCTION MEETING WITH THE EROSION CONTROL INSPECTOR.
- INSTALL GRAVEL CONSTRUCTION ENTRANCE AND SILT FENCE, COMMENCE CLEARING AND ROUGH GRADING OPERATIONS TO ESTABLISH DIVERSION SWALES AND SEDIMENT TRAPS AND BASIN.
- ROUGH GRADE SITE AND INSTALL UNDERGROUND UTILITIES.
- PROVIDE TEMPORARY PROTECTION MEASURES AND DEVICES AS REQUIRED UNTIL UNDERGROUND UTILITIES AND PAVEMENT SECTION IS COMPLETE AND FINAL GRADES ARE STABILIZED WITH TEMPORARY VEGETATIVE COVER.
- STABILIZE (FERTILIZE, SEED, AND MULCH) ALL DISTURBED AREAS AS SOON AS FINAL GRADES ARE ESTABLISHED.
- ONCE CONSTRUCTION IS COMPLETE AND ALL DISTURBED AREAS ARE STABILIZED, REMOVE TEMPORARY EROSION CONTROL MEASURES.

EROSION CONTROL MAINTENANCE PLAN

- ALL EROSION AND SEDIMENT CONTROL PRACTICES WILL BE CHECKED FOR STABILITY AND OPERATION FOLLOWING EVERY RUNOFF - PRODUCING RAINFALL BUT IN NO CASE LESS THAN ONCE EVERY WEEK. ANY NEEDED REPAIRS WILL BE MADE IMMEDIATELY TO MAINTAIN ALL PRACTICES AS DESIGNED.
- INSPECT SEDIMENT TRAPS AT LEAST WEEKLY AND AFTER EACH SIGNIFICANT RAINFALL EVENT AND REPAIR IMMEDIATELY. REMOVE SEDIMENT, AND RESTORE THE TRAP TO ITS ORIGINAL DIMENSIONS WHEN THE SEDIMENT HAS ACCUMULATED TO ONE-HALF THE DESIGN DEPTH OF THE TRAP. PLACE THE SEDIMENT THAT IS REMOVED IN DESIGNATED DISPOSAL AREA, AND REPLACE THE PART OF THE GRAVEL FACING THAT IS IMPAIRED BY SEDIMENT.
- SEDIMENT WILL BE REMOVED FROM BEHIND THE SEDIMENT FENCE WHEN IT BECOMES ABOUT 0.5 FEET DEEP AT THE FENCE. THE SEDIMENT FENCE WILL BE REPAIRED OR REPLACED AS NECESSARY TO MAINTAIN A BARRIER.
- ALL AREAS WILL BE FERTILIZED, RESEED, AND MULCHED AS NECESSARY, AND MULCHED ACCORDING TO SPECIFICATIONS IN THE VEGETATIVE PLAN TO MAINTAIN A VIGOROUS, DENSE VEGETATIVE COVER.
- GRAVEL CONSTRUCTION ENTRANCE TO BE CLEANED WHEN SEDIMENT ACCUMULATIONS ARE VISIBLE OR SEDIMENT IS DEPOSITED ON THE ASPHALT AND STONE WILL BE PERIODICALLY TOP DRESSED WITH 2 INCHES OF #4 STONE TO MAINTAIN 6 INCH DEPTH.
- INSPECT INLETS AT LEAST WEEKLY AND AFTER EACH SIGNIFICANT (1" OR GREATER) RAINFALL EVENT. CLEAR THE WIRE OF ANY DEBRIS OR OTHER OBJECTS TO PROVIDE ADEQUATE FLOW FOR SUBSEQUENT RAINS. TAKE CARE NOT TO DAMAGE OR UNDERCUT THE WIRE MESH DURING SEDIMENT REMOVAL. REPLACE STONE AS NEEDED.

EROSION CONTROL NOTES

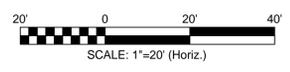
- THIS PROJECT DOES NOT QUALIFY FOR A NHC S&E PERMIT. REGARDLESS CONTRACTOR SHALL FOLLOW GOOD PRACTICE TECHNIQUES.
- CONTRACTOR SHALL REMOVE ALL TREES AND VEGETATION WITHIN ROAD RIGHT OF WAYS UNLESS OTHERWISE DESIGNATED TO REMAIN.
- CONTRACTOR SHALL RAKE AND REMOVE ROOTS, STUMPS, VEGETATION, DEBRIS, EXISTING STRUCTURES ABOVE AND BELOW GRADE, ORGANIC MATERIAL OR ANY OTHER UNSUITABLE MATERIAL WITH IN THE LIMITS OF CONSTRUCTION.
- CONTRACTOR SHALL COORDINATE WITH OWNER AND THEIR GEOTECHNICAL REPRESENTATIVE TO COORDINATE REMOVAL OF ANY UNSUITABLE SOILS.
- CLEARED, GRUBBED, STRIPPED OR OTHER WASTE MATERIALS SHALL BE REMOVED FROM THE SITE AND DISPOSED OF IN A PROPERLY PERMITTED FACILITY.
- CONTRACTOR SHALL FURNISH ANY REQUIRED BORROW MATERIALS FROM A PROPERLY PERMITTED OFF-SITE FACILITY.
- ALL GRADED SLOPES MUST BE SEEDED AND MULCHED WITHIN 21 CALENDAR DAYS OF COMPLETION OF GRADING. STABILIZE AREAS OTHER THAN SLOPES WITHIN 15 WORKING DAYS OR 90 CALENDAR DAYS, WHICHEVER IS SHORTER.
- ADDITIONAL MEASURES TO CONTROL EROSION AND SEDIMENT MAY BE REQUIRED BY A REPRESENTATIVE OF THE COUNTY ENGINEERING DEPARTMENT.
- SLOPES SHALL BE GRADED NO STEEPER THAN 3:1.
- ADDITIONAL DEVICES MAY BE REQUIRED AS AGREED UPON BY THE FIELD INSPECTOR, ENGINEER, AND OWNER.
- IF ACTIVE CONSTRUCTION CEASES IN ANY AREA FOR MORE THAN 15 CALENDAR DAYS, ALL DISTURBED AREAS MUST BE SEEDED, MULCHED, AND TACKED.

LABEL	1) GROUND STABILIZATION	STABILIZATION TIME FRAME	STABILIZATION TIME FRAME EXCEPTIONS
[Cross-hatch pattern]	PERIMETER DIKES, DITCHES AND SLOPES	7 DAYS	NONE
	HIGH QUALITY WATER (HOW) ZONES	7 DAYS	NONE
	SLOPES STEEPER THAN 3:1	7 DAYS	IF SLOPES ARE 10' OR LESS IN LENGTH AND ARE NOT STEEPER THAN 2:1:14 DAYS ARE ALLOWED
[Diagonal line pattern]	SLOPES 3:1 OR FLATTER	14 DAYS	7 DAYS FOR SLOPES GREATER THAN 50 FEET IN LENGTH
	ALL OTHER AREAS WITH SLOPES FLATTER THAN 4:1	14 DAYS	NONE (EXCEPT FOR PERIMETERS AND HOW ZONES)

- 2) BUILDING WASTES HANDLING
- NO PAINT OR LIQUID WASTES IN STREAM OF STORM DRAINS
 - DEDICATED AREAS FOR DEMOLITION, CONSTRUCTION AND OTHER WASTES MUST BE LOCATED 50' FROM STORM DRAINS AND STREAMS UNLESS NO REASONABLE ALTERNATIVES AVAILABLE
 - EARTHEN-MATERIALS STOCKPILES MUST BE LOCATED 50' FROM STORM DRAINS AND STREAMS UNLESS NO REASONABLE ALTERNATIVES AVAILABLE
 - CONCRETE MATERIALS MUST BE CONTROLLED TO AVOID CONTACT WITH SURFACE WATERS, WETLANDS, OR BUFFERS.
- 3) INSPECTIONS
- SAME WEEKLY INSPECTION REQUIREMENTS
 - SAME RAIN GAUGE & INSPECTIONS AFTER 0.5" RAIN EVENT
 - INSPECTIONS ARE ONLY REQUIRED DURING "NORMAL BUSINESS HOURS"
 - INSPECTION REPORTS MUST BE AVAILABLE ON-SITE DURING BUSINESS HOURS UNLESS A SITE-SPECIFIC EXEMPTION IS APPROVED
 - RECORDS MUST BE KEPT FOR 3 YEARS AND AVAILABLE UPON REQUEST
 - ELECTRONICALLY-AVAILABLE RECORDS MAY BE SUBSTITUTES UNDER CERTAIN CONDITIONS
- 4) SEDIMENT BASINS
- OUTLET STRUCTURES MUST WITHDRAW FROM BASIN SURFACES UNLESS DRAINAGE AREA IS LESS THAN 1 ACRE
 - USE ONLY DWQ-APPROVED FLOCCULANTS

NPDES - SPECIFIC PLAN SHEET NOTES

- THIS PAGE IS SUBMITTED TO COMPLY WITH NPDES GENERAL STORMWATER PERMIT N0010000.
- THIS PAGE CAN BE APPROVED BY THE COUNTY PURSUANT TO NPDES GENERAL STORMWATER PERMIT N0010000 ONLY.
- THIS PAGE OF THE APPROVED PLANS IS ENFORCEABLE EXCLUSIVELY PURSUANT TO NPDES GENERAL STORMWATER PERMIT N0010000.
- THE COUNTY IS NOT AUTHORIZED TO ENFORCE THIS PAGE OF THE PLANS AND IT IS NOT PART OF THE APPROVED PLANS FOR THE PURPOSES OF ENFORCEMENT ACTION UNDER THE CITY CODE.



GRADING, DRAINAGE & EROSION CONTROL
SCALE 1" = 20'

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Approved Construction Plan

Name	Date
Planning	
Traffic	
Fire	

DRAINAGE PLAN APPROVED
CITY OF WILMINGTON

STORMWATER DISCHARGE PERMIT NOT REQUIRED

SIGNATURE _____ DATE _____



LEGEND

- ASPHALT PAVEMENT [Solid grey box]
- CONC. PAVEMENT [Stippled box]
- EXISTING SPOT ELEVATION [Crosshair symbol] 41.5'
- PROPOSED SPOT ELEVATION [Dot symbol] 31.0'
- WAVE STYLE BIKE RACK 8 BIKE SPOTS EACH [Wave pattern box]
- TREE PROTECTION FENCING SEE DETAIL [Square with dashed border]

NUMBER	DATE	REVISION

DESIGN BY: BDS
DRAWN BY: BDS
CHECKED BY: BDS
DATE: AUGUST, 2106

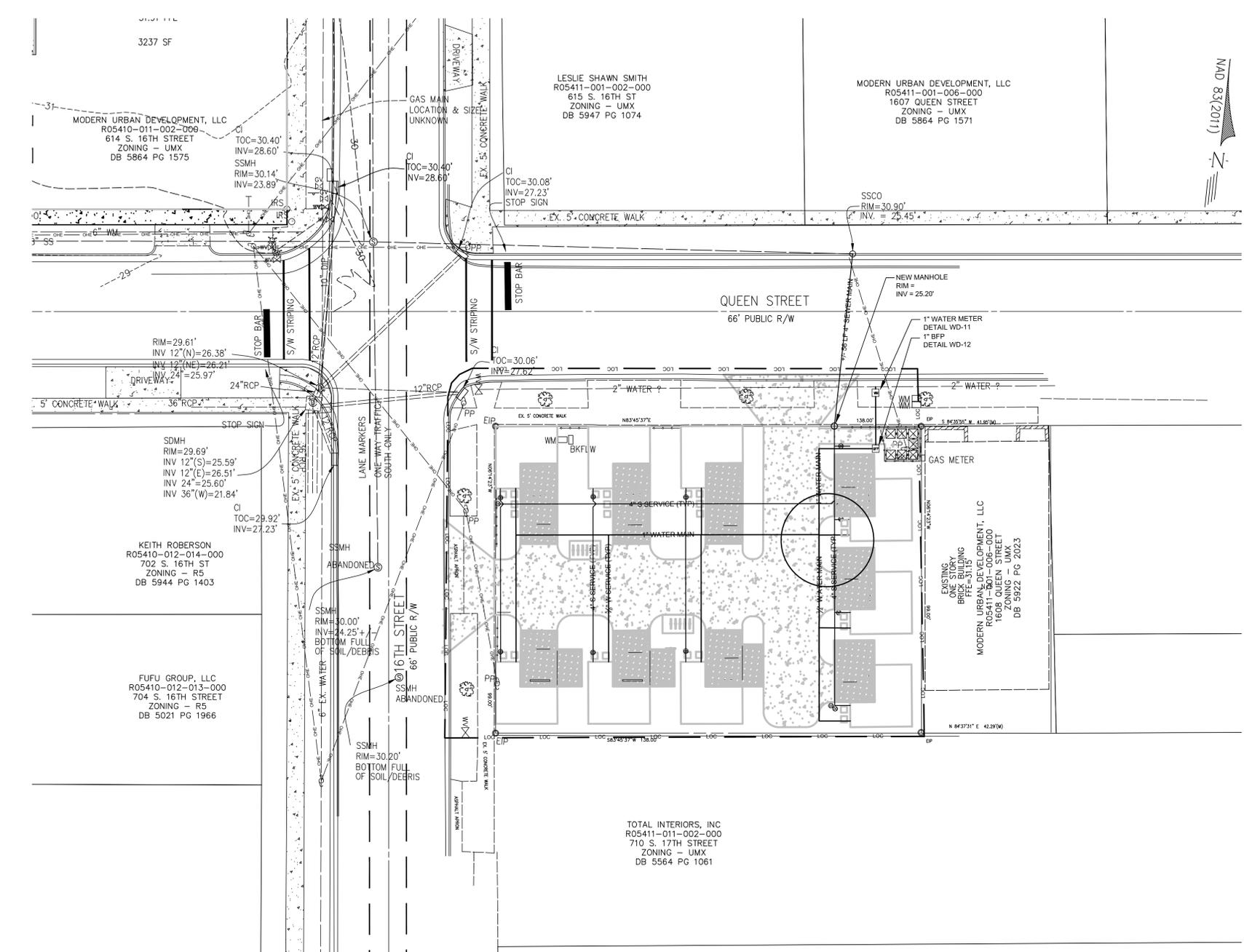
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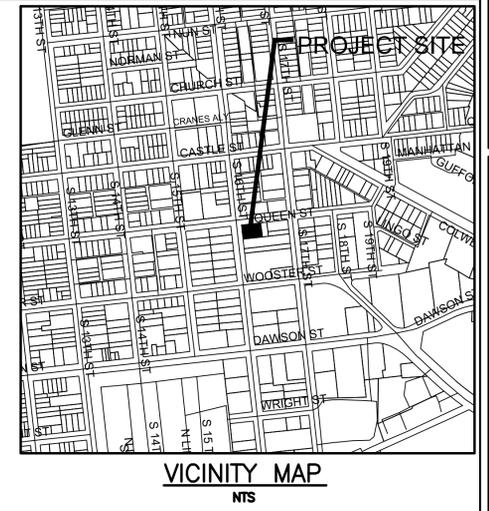
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PLANNING - ENGINEERING - PROJECT MANAGEMENT

SHEET
C2
of X
JOB NO. 104-002



UTILITES
UTILITY INFORMATION

WATER EXISTING USAGE	0 GPD
WATER USAGE PROPOSED (9 - 1 BDRM UNITS BUT MINIMUM 240 GPD USAGE)	2,160 GPD
SEWER USAGE - EXISTING	0 GPD
SEWER USAGE - PROPOSED	2,160 GPD



DESIGNED BY:	BDS
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UTILITY PLAN
SQUARE TWO
WILMINGTON, NC

L S SMITH, INC.
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Wilmington, NC 28401-5526
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SCALE 1" = 20'

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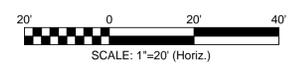
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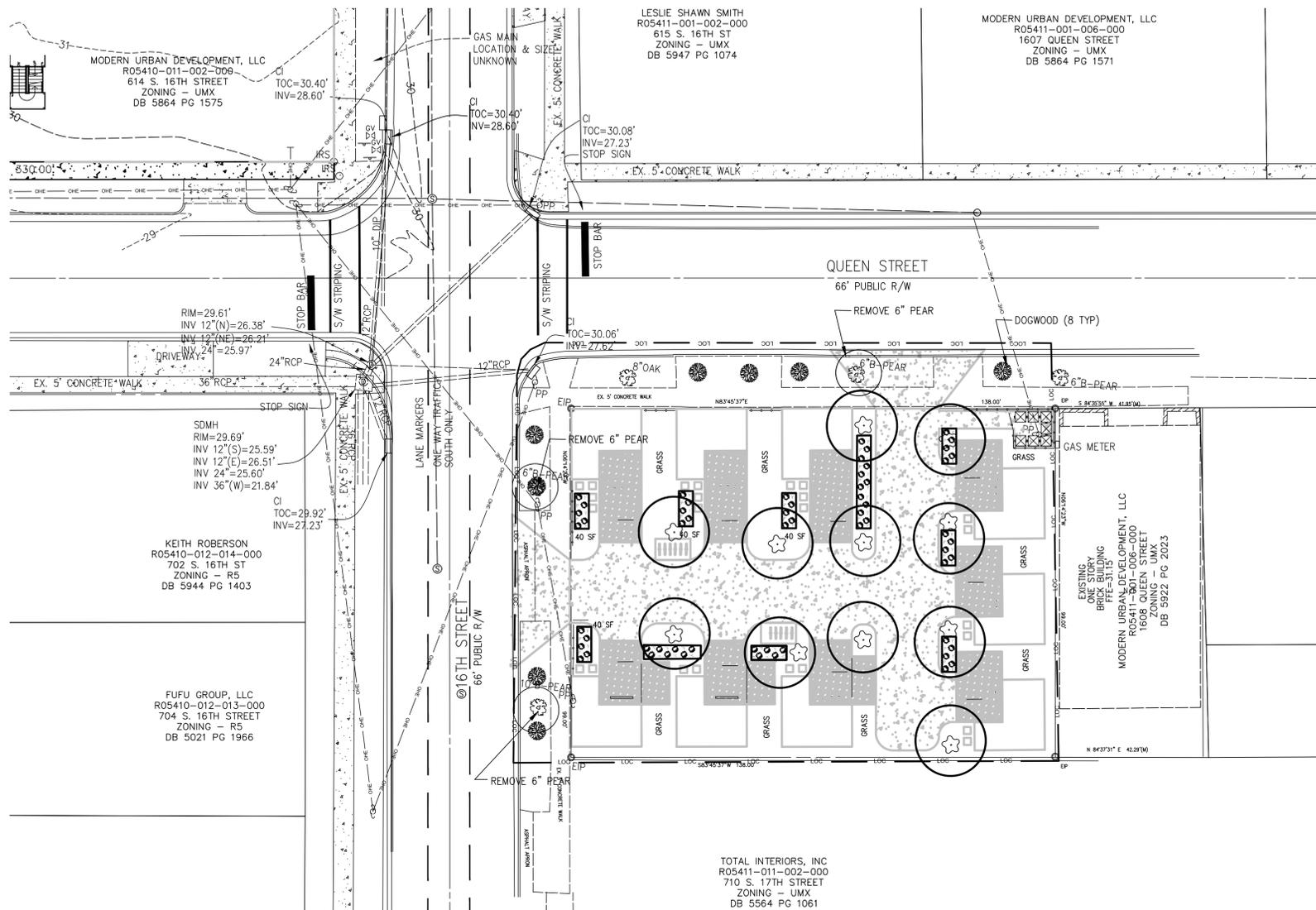
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DRAINAGE PLAN
APPROVED
CITY OF WILMINGTON

STORMWATER DISCHARGE
PERMIT NOT REQUIRED

SIGNATURE _____ DATE _____





LANDSCAPE PLAN
SCALE 1" = 20'

LANDSCAPE DATA

STREET YARD BUFFER
1 TREE PER 30' FRONTAGE 237' FRONTAGE
237 LF FRONTAGE = 8 TREES REQUIRED - 8 PROPOSED
1 EXISTING

PARKING LOT LANDSCAPING
PARKING LOT SHADING WILL BE ACCOMPLISHED WITH PROPOSED TREES SHOWN ON PLAN. PER COW CRITERIA THE PARKING LOT SHALL HAVE A 20% SHADING COVERAGE.
PARKING & ISLES ON SITE EQUAL 6,829 SF X 20% = 1,366 SF
SHADING FROM PROPOSED TREES 10 WILLOW OAKS WITH 10' RADIUS = 314 SF X 10 = 3,140 SF OF SHADING. MEETS ORDINANCE.

PERIMETER LANDSCAPING - NOT APPLICABLE FOR THIS PROJECT

FOUNDATION PLANTINGS
HEIGHT X WIDTH X 12%
17' X 8' X 12% = 17 SF
EACH UNIT HAS 40 SF LANDSCAPE BED
PROPOSED EXCEEDS ORDINANCE

PLAN SHOWS PROPOSED PLANTINGS, SHRUBS OTHER THAN SHOWN MAY BE SUBSTITUTED WITH SELECTIONS FROM THE APPROVED PLANTING LIST AS SHOWN IN THE MOST CURRENT COW TECHNICAL STANDARDS AND SPECIFICATIONS MANUAL

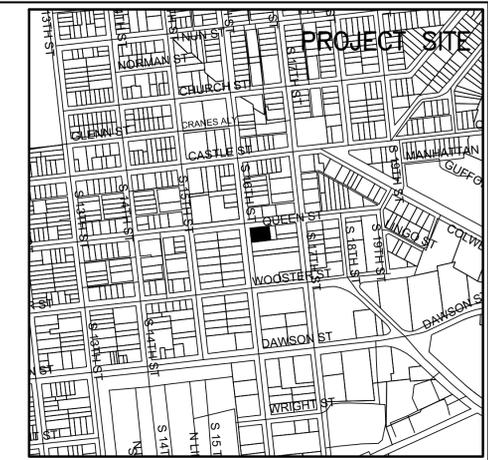
BUFFER YARDS
NOT APPLICABLE FOR THIS PROJECT

GROUND COVER
ALL ISLAND SHALL BE PLANTED WITH GROUND COVER PLANTING SHOWN IN THE APPROVED PLANT LIST MENTIONED ABOVE OR FINE STRAW/MULCH

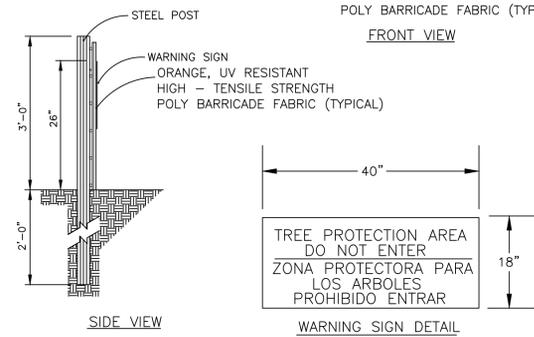
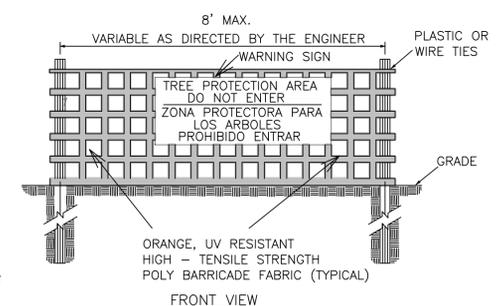
LANDSCAPE NOTES

- ALL PLANTED AND RETAINED LIVING MATERIAL, REQUIRED TO MEET THE PROVISIONS OF THE CITY OF WILMINGTON, LAND DEVELOPMENT CODE, SHALL BE PERPETUALLY PROTECTED AND MAINTAINED, TO PROFESSIONALLY ACCEPTED STANDARDS BY JOINT AND SEVERAL RESPONSIBILITY OF THE OWNER, OCCUPANTS, TENANT AND RESPECTIVE LIVE AGENTS OF THE PROPERTY ON WHICH THE MATERIAL IS LOCATED.
- LANDSCAPING SHOWN IS MINIMAL AMOUNT OF LANDSCAPING PER THE UMX ZONING. OWNER WILL PLANT MORE MATERIAL IN LANDSCAPE BEDS AS SHOWN.
- INSTALL TREE PROTECTION AT A RATE OF 1' OF FENCING PER 1" DIA AT DBH.

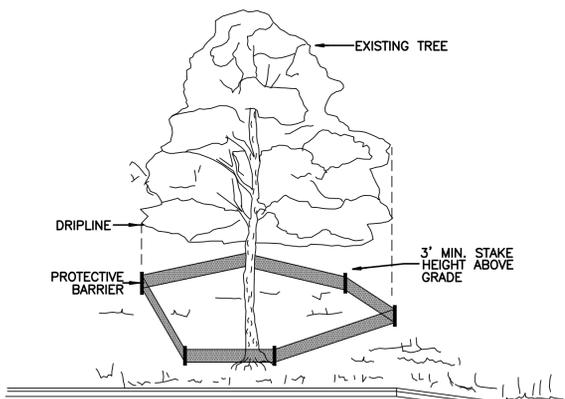
STREET TREES				
Scientific Name	Common Name	Size	Symbol	Quantity
CORNUS FLORIDA	DOGWOOD	8' HEIGHT	●	26
QUERCUS PHELLOS	WILLOW OAK	MIN. 2" CALIPER	☆	19
ILEX VOMITORIA	DWARF YALPON HOLLY	9' GALLON / SHRUB	● Y	100
PHYTOSPORUM TOPEA	DWARF PHYTOSPORUM	9' GALLON / SHRUB	● P	100



VICINITY MAP
NTS



- NOTES:
- THE TREE PROTECTION FENCING SHALL NOT BE VIOLATED FOR THE ENTIRE DURATION OF THE PROJECT WITHOUT APPROVAL FROM URBAN FORESTRY STAFF.
 - WARNING SIGNS TO BE MADE OF DURABLE, WEATHERPROOF MATERIAL. LETTERS TO BE 3" HIGH, MINIMUM, CLEARLY LEGIBLE AND SPACED AS DETAILED.
 - SIGNS SHALL BE PLACED AT 50' MAXIMUM INTERVALS. PLACE A SIGN AT EACH END OF LINEAR TREE PROTECTION AND 50' ON CENTER THEREAFTER. FOR TREE PROTECTION AREAS LESS THAN 100' IN PERIMETER, PROVIDE NO LESS THAN TWO SIGNS PER PROTECTION AREA.
 - ATTACH SIGNS SECURELY TO FENCE POSTS AND FABRIC. MAINTAIN TREE PROTECTION FENCE AND SIGNS THROUGHOUT DURATION OF PROJECT.
 - TREE PROTECTION FENCING AND SIGNAGE SHALL BE REMOVED AFTER CONSTRUCTION.
 - ADDITIONAL SIGNS MAY BE REQUIRED BY CITY OF WILMINGTON, BASED ON ACTUAL FIELD CONDITIONS.



TREES TO BE SAVED WILL BE CLEARLY MARKED PRIOR TO CONSTRUCTION AND A PROTECTIVE BARRIER IS TO BE INSTALLED AT THE DRIPLINE.
DRIPLINE - THE AREA OF SOIL DIRECTLY BENEATH THE TREE EXTENDING OUT TO THE TIPS OF THE OUTERMOST BRANCHES.
CONTRACTOR TO COMPLY WITH COW SIGN ORDINANCE FOR MARKING FENCING

METHOD OF TREE PROTECTION DURING CONSTRUCTION
SD 15-08
NOT TO SCALE

TREE PROTECTION DURING CONSTRUCTION
SD 15-09
NOT TO SCALE

DESIGN BY: BDS
DRAWN BY: BDS
CHECKED BY: BDS
DATE: AUGUST, 2016

LANDSCAPE PLANS
SQUARE TWO
WILMINGTON, NC

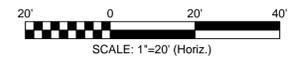
L S SMITH, INC.
1607 Queen Street
Wilmington, NC 28401-5526
910-409-8782

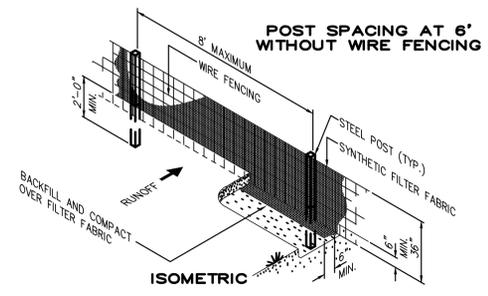


JBS CONSULTING, PA
7332 Cotesworth Drive
Wilmington, NC 28405
(910) 619-9990
License Number C-2825
PLANNING - ENGINEERING - PROJECT MANAGEMENT

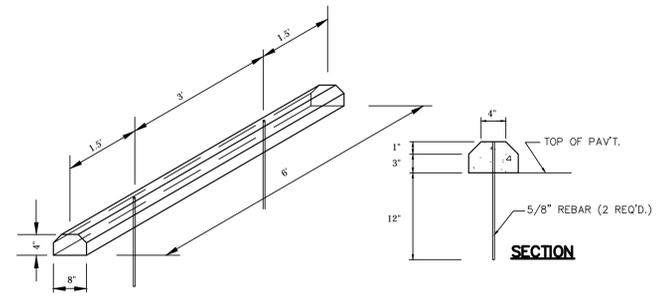
SHEET
C4
JOB NO. 104-001

NUMBER	DATE	REVISION

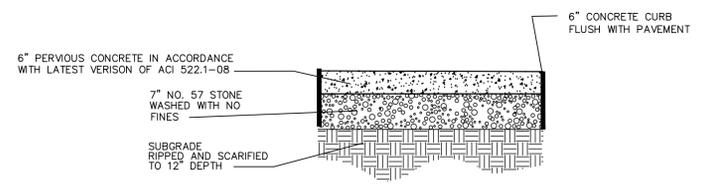




SILT FENCE DETAIL
NOT TO SCALE

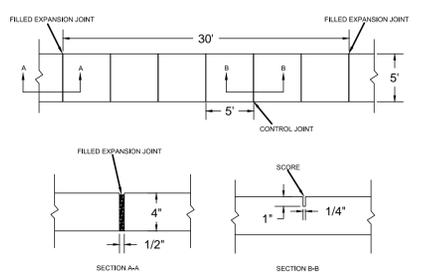


WHEEL STOP DETAIL
NOT TO SCALE



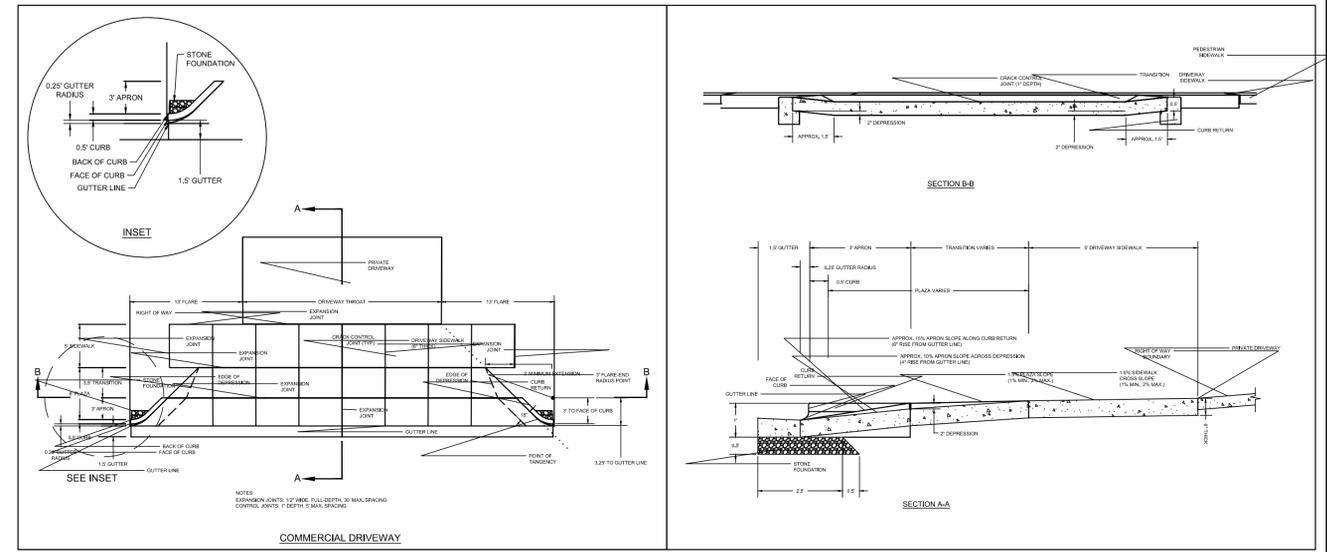
PERVIOUS CONCRETE PAVEMENT SECTION
NOT TO SCALE

NOTE TO CONTRACTOR
DURING CONSTRUCTION, PRIOR TO INSTALLATION OF THE SUB-GRADE, INFILTRATION/HYDRAULIC CONDUCTIVITY TESTING MUST BE PERFORMED IN THE SURFACE OF THE UNDERLYING SOIL. TWO TEST SPOTS, RANDOM SHALL BE PERFORMED. RESULTS OF TESTING TO BE REVIEWED BY CIVIL ENGINEER PRIOR TO THE INSTALLATION OF THE SUB-GRADE STONE.

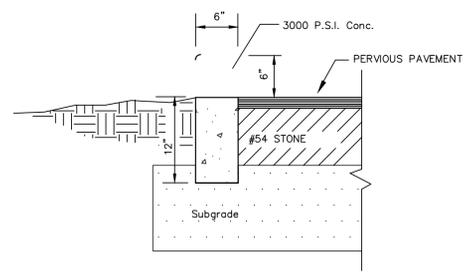


- NOTES:**
1. JOINT MATERIAL TO COMPLY WITH CURRENT NCDOT STANDARDS.
 2. SANITARY SEWER CLEAN-OUTS, WATER METERS, MANHOLES, AND VALVE LIDS TO BE LOCATED OUTSIDE SIDEWALK WHERE FEASIBLE.
 3. MINIMUM SIDEWALK WIDTH TO BE 6' MINIMUM IF PLACED AT BACK OF CURB.
 4. CONCRETE FOR ALL SIDEWALKS (EXCEPT ANY PORTION CONTAIN WITHIN A DRIVEWAY APRON) SHALL BE CLASS "A" - 3,000 PSI.
 5. MINIMUM REPLACEMENT FOR REPAIRS IS A 6' X 6' PANEL.
 6. 4" STONE BASE MAY BE REQUIRED FOR POOR SOIL CONDITIONS.
 7. MINIMUM DEPTH FOR TUNNELING BELOW SIDEWALK IS 12"
 8. MAX ADJACENT GROUND SLOPE WITHOUT RAILING IS 2:1
 9. MIN GRADE FOR PROPER DRAINAGE IS 1% IN AT LEAST 1 DIRECTION. MAX CROSS SLOPE IS 2%. MAX LONGITUDINAL SLOPE IS 8.3%, 10% IF LIMITED BY EXISTING CONDITIONS OR NO GREATER THAN THE SLOPE OF THE EXISTING ADJACENT ROAD.

CONCRETE WALK SECTION
SD 9-10



COMMERCIAL DRIVEWAY



HEADER CURB DETAIL
NOT TO SCALE

Approved Construction Plan

Name _____ Date _____

Planning _____

Traffic _____

Fire _____

DRAINAGE PLAN APPROVED
CITY OF WILMINGTON

STORMWATER DISCHARGE PERMIT NOT REQUIRED

SIGNATURE _____ DATE _____

For each open utility cut of City streets, a \$325 permit shall be required from the City prior to occupancy and/or project acceptance.

NUMBER	DATE	REVISION

DESIGN BY:	BDS
DRAWN BY:	BDS
CHECKED BY:	BDS
DATE:	AUGUST, 2016

DETAILS
SQUARE TWO
WILMINGTON, NC

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